



DEPARTMENT OF THE ARMY
U.S. ARMY CORPS OF ENGINEERS, NEW ORLEANS DISTRICT
7400 LEAKE AVENUE
NEW ORLEANS, LA 70118-3651

October 2, 2023

Regulatory Division
Central Evaluation Branch

Project Manager:
Jamie Crowe
(504) 862-2675
Jamie.M.Crowe@usace.army.mil
Application #: MVN 2022-00750-CO

PUBLIC NOTICE

Interested parties are hereby notified that a permit application has been received by the New Orleans District of the US Army Corps of Engineers pursuant to: [X] Section 10 of the Rivers and Harbors Act of March 3, 1899 (30 Stat. 1151; 33 USC 403), and [X] Section 404 of the Clean Water Act (86 Stat. 816; 33 USC 1344).

PROPOSED BARGE DOCK IN THE MISSISSIPPI RIVER IN IBERVILLE PARISH

NAME OF APPLICANT: Willow Glen Terminals, c/o Lanier & Associates Consultants, 4101 Magazine Street, New Orleans, Louisiana .

LOCATION OF WORK: In the Mississippi River, left descending bank, mile 201.8, near St. Gabriel, Louisiana, in Iberville Parish, within the Mississippi River Basin in hydrologic unit (HUC 08070100), as shown on the attached drawings.

CHARACTER OF WORK: The applicant has requested a Department of the Army authorization to install a new dock barge, approach way, levee pipe crossing, River Road crossing, and pipe supports. A 100' wide construction corridor will be cleared and matted during construction for equipment access. Upon completion, mats will be removed and pre-project contours restored. Approximately 100 cubic yards of rip rap, 260 cubic yards of hauled-in fill material, and 200 cubic yards of concrete will be installed within 0.01 acres of non-vegetated water bottoms and 0.5 acre of jurisdictional batture. Approximately 1 acre of forested batture wetlands will be impacted by project implementation.

MITIGATION: The applicant states that the proposed project has been designed to avoid and minimize direct and secondary adverse impacts to the maximum extent practicable. Any further reduction in footprint would not allow for safe equipment access during construction. As compensation for unavoidable wetland impacts, the applicant proposes to use a Corps approved mitigation bank within the watershed of impact.

The comment period for the requested Department of Army will close in **30 days** from

the date of this public notice. Written comments, including suggestions for modifications or objections to the proposed work, stating reasons thereof, are being solicited from anyone having interest in this permit request, and must be submitted so as to be received before or by the last day of the comment period. Letters and/or comments concerning the subject permit application must reference the Applicant's Name and the Permit Application Number and can be preferably emailed to the Corps of Engineer's project manager listed above or forwarded to the Corps of Engineers at the address above, **ATTENTION: REGULATORY DIVISION, RGC, Jamie Crowe**. This public notice is also available for review online at <https://go.usa.gov/xennJ>.

Corps of Engineers Permit Criteria

The decision whether to issue a permit will be based on an evaluation of the probable impacts, including cumulative impacts of the proposed activity on the public interest. That decision will reflect the national concern for both protection and utilization of important resources. The benefit which reasonably may be expected to accrue from the proposal must be balanced against its reasonably foreseeable detriments. All factors which may be relevant to the proposal will be considered including the cumulative effects thereof; among those are conservation, economics, aesthetics, general environmental concerns, wetlands, historic properties, fish and wildlife values, flood hazards, floodplain values, land use, navigation, shoreline erosion and accretion, recreation, water supply and conservation, water quality, energy needs, safety, food and fiber production, mineral needs, considerations of property ownership and, in general, the needs and welfare of the people.

The U.S. Army Corps of Engineers is soliciting comments from the public, federal, state, and local agencies and officials, Indian Tribes, and other interested parties in order to consider and evaluate the impacts of this proposed activity. Any comments received will be considered by the U.S. Army Corps of Engineers to determine whether to make, modify, condition, or deny a permit for this proposal. To make this decision, comments are used to assess impacts on endangered species, historic properties, water quality, general environmental effects, and other public interest factors listed above. Comments are used in the preparation of an Environmental Assessment and/or an Environmental Impact Statement pursuant to the National Environmental Policy Act. Comments are also used to determine the need for a public hearing and to determine the overall public interest of the proposed activity. Further, all factors that may be relevant to the proposal will be considered, including the potential cumulative effects associated with the proposed project.

The New Orleans District is presently unaware of properties listed on the National Register of Historic Places near the proposed work. The possibility exists that the proposed work may damage or destroy presently unknown archeological, scientific, prehistorical, historical sites, or data. As deemed necessary, copies of this public notice will be sent to the State Archeologist, State Historic Preservation Officer, and federally listed tribes regarding potential impacts to cultural resources.

Our initial finding is that the proposed work would have no effect on any species listed as endangered by the U.S. Department of Commerce, nor affect any habitat designated as critical to the survival and recovery of any endangered species.

The New Orleans District has determined that the proposed project is not likely to adversely affect the pallid sturgeon (*Scaphirhynchus albus*), and will have no effect on the west Indian manatee (*Trichechus manatus*). No other known endangered/threatened species will be impacted by the proposed activity. These determinations are based on Information & Planning Consultation for Endangered Species in Louisiana (IPaC) agreement dated January 27, 2020 between the US Army Corps of Engineers, New Orleans District and US Fish and Wildlife Service, Ecological Services Office.

This notice initiates the Essential Fish Habitat (EFH) consultation requirements of the Magnuson-Stevens Fishery Conservation and Management Act. The applicant's proposal would result in the destruction or alteration of up to n/a acres of EFH utilized by various life stages of red drum and penaeid shrimp. Our initial determination is that the proposed action would not have a substantial adverse impact on EFH or federally managed fisheries in the Gulf of Mexico. Our final determination relative to project impacts and the need for mitigation measures is subject to review by and coordination with the National Marine Fisheries Service.

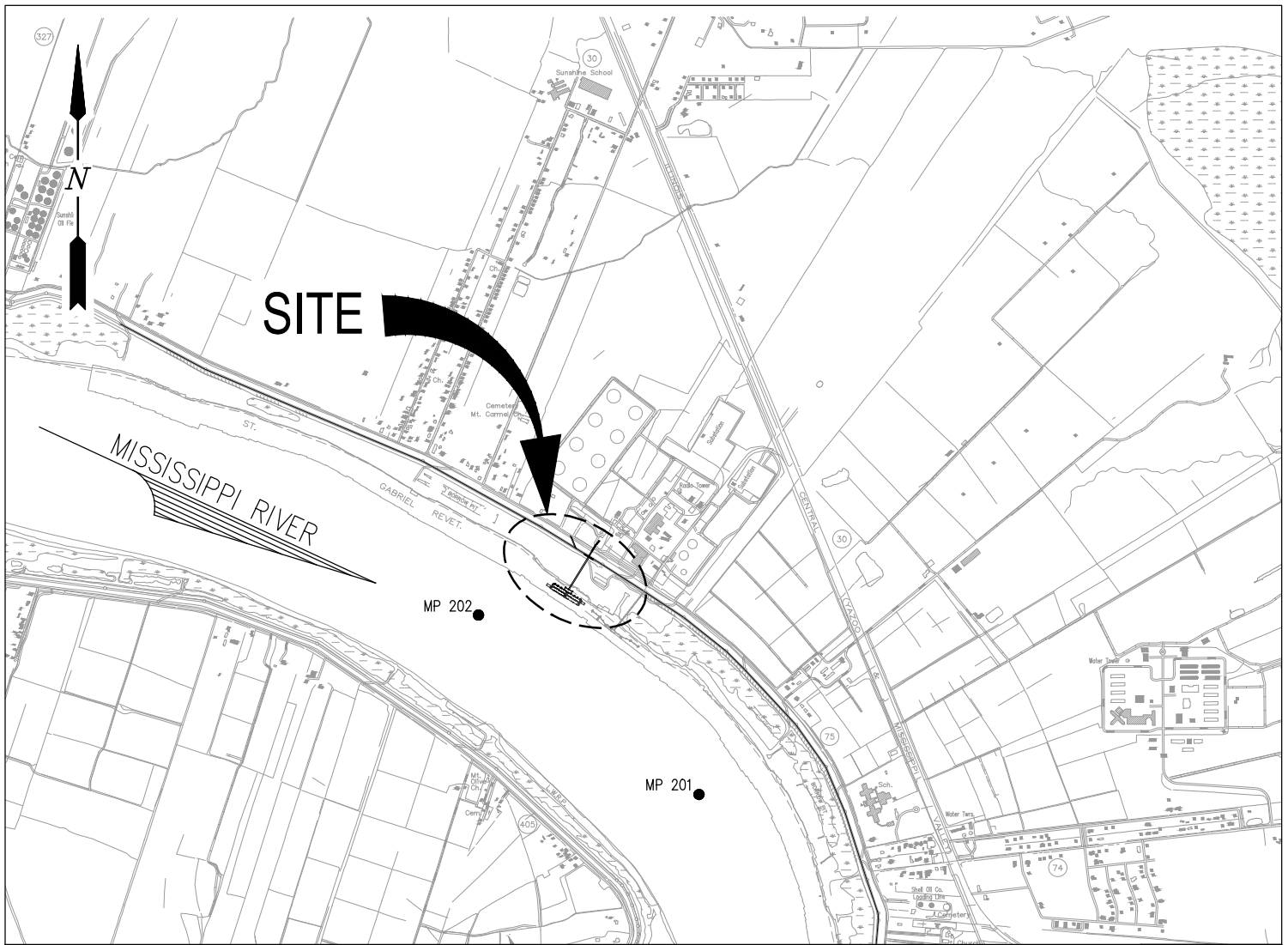
If the proposed work involves deposits of dredged or fill material into navigable waters, the evaluation of the probable impacts will include the application of guidelines established by the Administrator of the Environmental Protection Agency. Also, a certification that the proposed activity will not violate applicable water quality standards will be required from the Department of Environmental Quality before a permit is issued.

Any person may request, (preferably by email to the project manager, or in writing), within the comment period specified in this notice, that a public hearing be held to consider this application. Requests for public hearings shall state, with particularity, the reasons for holding a public hearing.

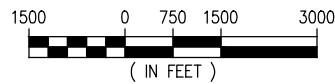
You are invited to communicate the information contained in this notice to any other parties whom you deem likely to have interest in the matter.

John M. Herman
Chief, Central Evaluation Branch
Regulatory Division

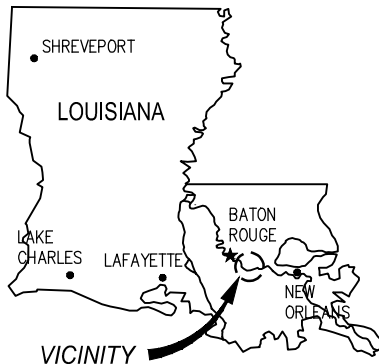
Enclosures



VICINITY MAP



IBERVILLE PARISH
LAT: 30° 16' 16"N
LONG: 91° 07' 15"W



AREA MAP

NOTES

1. AS-BUILT DRAWINGS AND/OR PLANS SHALL HAVE WRITTEN ON THEM THE DATE OF COMPLETION OF SAID ACTIVITIES AND SHALL BE SUBMITTED TO THE LOUISIANA DEPARTMENT OF NATURAL RESOURCES, OFFICE OF COASTAL MANAGEMENT, P.O. BOX 44487, BATON ROUGE, LA 70804-4487 WITHIN 30 DAYS FOLLOWING PROJECT COMPLETION.
2. ALL STRUCTURES, FACILITIES, WELL AND PIPELINES/FLOWLINES OCCURRING IN OPEN WATER AREAS OR IN OILFIELD CANALS OR SLIPS SHALL BE REMOVED WITHIN 120 DAYS OF ABANDONMENT OF THE FACILITIES FOR THE HEREIN PERMITTED USE UNLESS PRIOR WRITTEN APPROVAL TO LEAVE SUCH STRUCTURES IN PLACE IS RECEIVED FROM THE COASTAL MANAGEMENT DIVISION. THIS CONDITION DOES NOT PRECLUDE THE NECESSITY FOR REVISING THE CURRENT PERMIT OR OBTAINING A SEPARATE COASTAL USE PERMIT, SHOULD ONE BE REQUIRED.
3. STRUCTURES MUST ALSO BE MARKED/LIGHTED IN ACCORDANCE WITH U.S. COAST GUARD REGULATIONS.
4. IN ORDER TO ENSURE THE SAFETY OF ALL PARTIES, THE PERMITTEE SHALL CONTACT THE LOUISIANA ONE CALL SYSTEM (1-800-272-3020) A MINIMUM OF 48 HOURS PRIOR TO THE COMMENCEMENT OF ANY EXCAVATION (DIGGING, DREDGING, JETTING, ETC.) OR DEMOLITION ACTIVITY.
5. COAST GUARD SHALL BE NOTIFIED AT LEAST 48 HOURS PRIOR TO ANY WORK THAT WILL RESULT IN ANY WATERWAY BLOCKAGE THROUGHOUT THE CONSTRUCTION PROCESS.
6. ALL ELEVATIONS REFER TO NAVD88 UNLESS NOTED OTHERWISE.

BRADFORD N. FRISCHERTZ P.E.
LA PE #32628

PRELIMINARY - FOR PERMIT PURPOSES ONLY

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LANIER & ASSOCIATES CONSULTING ENGINEERS

INCORPORATED

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TX: F-2981

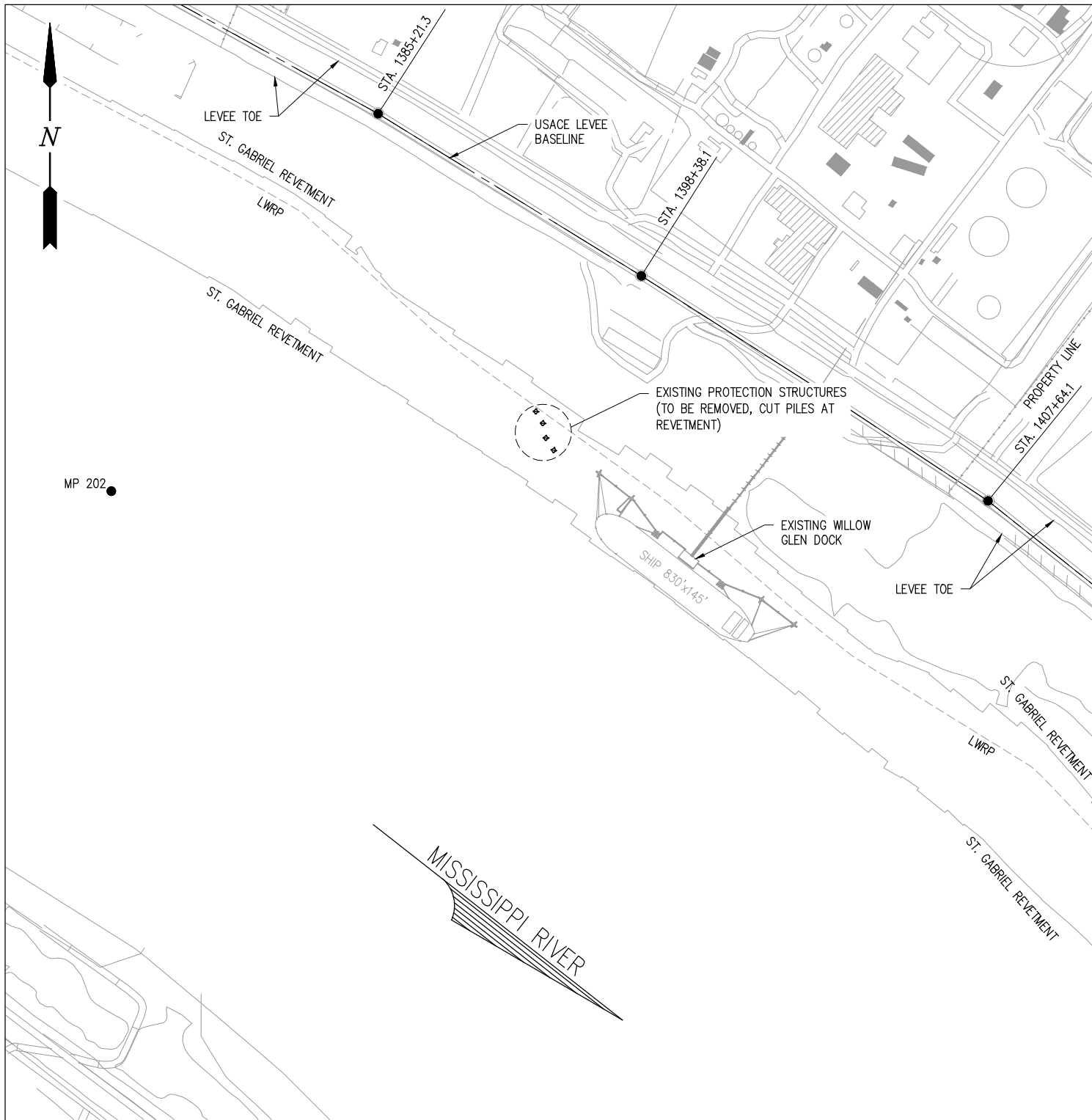
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WILLOW GLEN TERMINAL, LLC.
ST. GABRIEL LOUISIANA

PROPOSED NEW BARGE DOCK AREA MAP, VICINITY MAP & NOTES

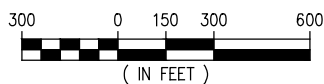
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EXISTING SITE PLAN

SCALE: 1" = 600'



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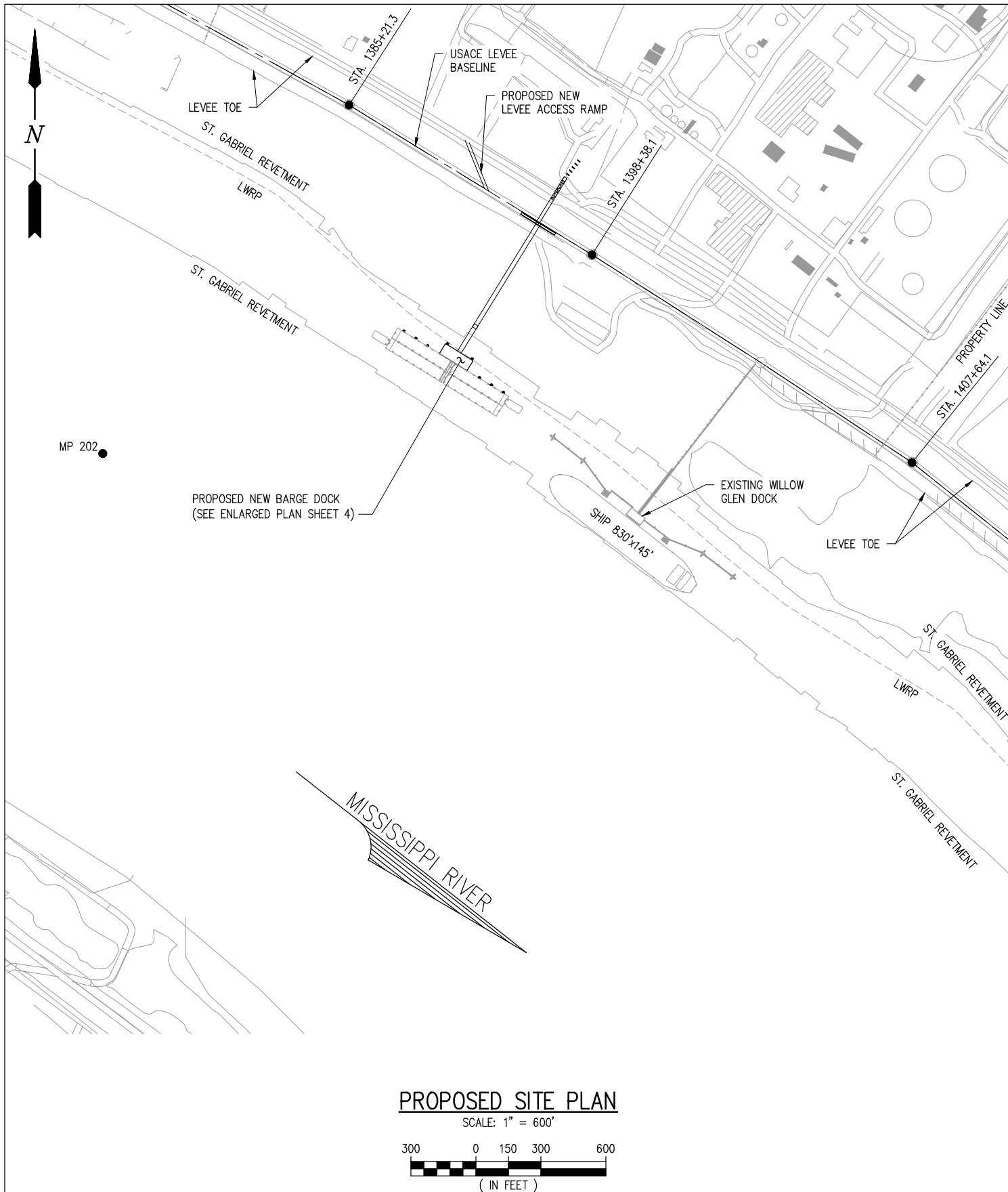
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PROPOSED NEW BARGE DOCK
EXISTING SITE PLAN

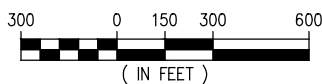
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PROPOSED SITE PLAN

SCALE: 1" = 600'



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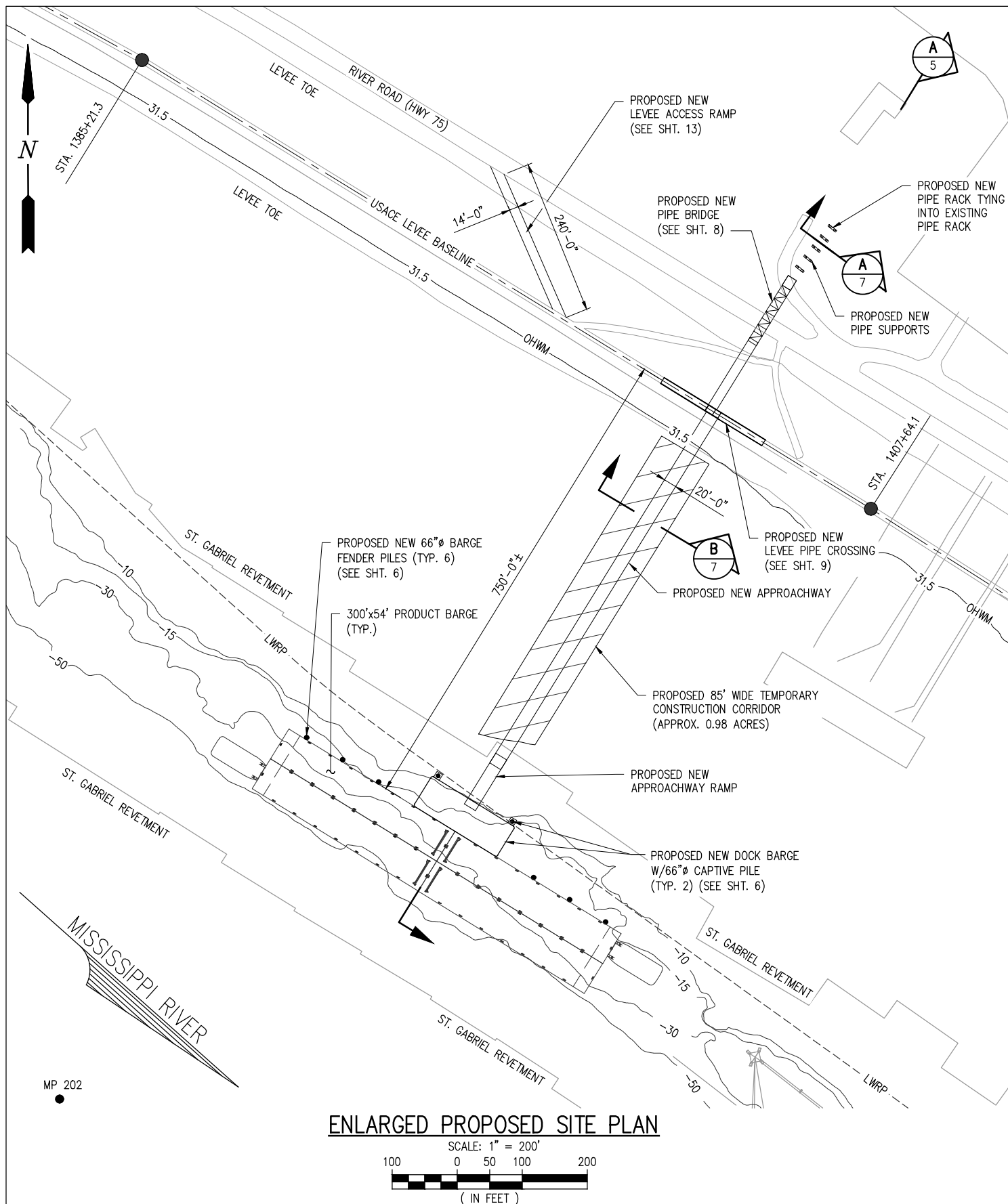
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PROPOSED SITE PLAN

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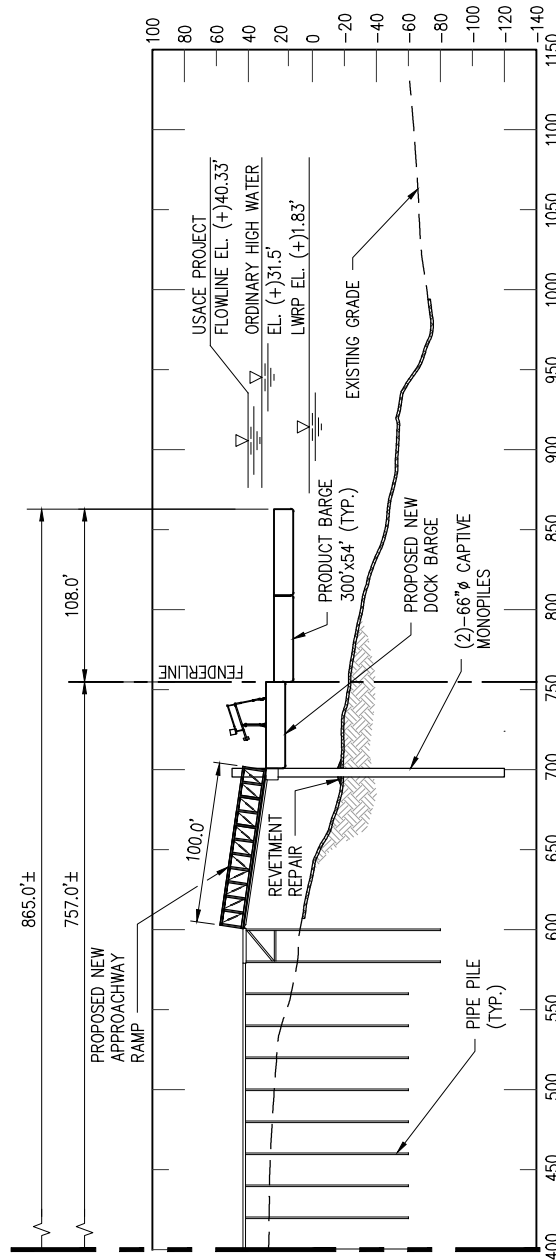
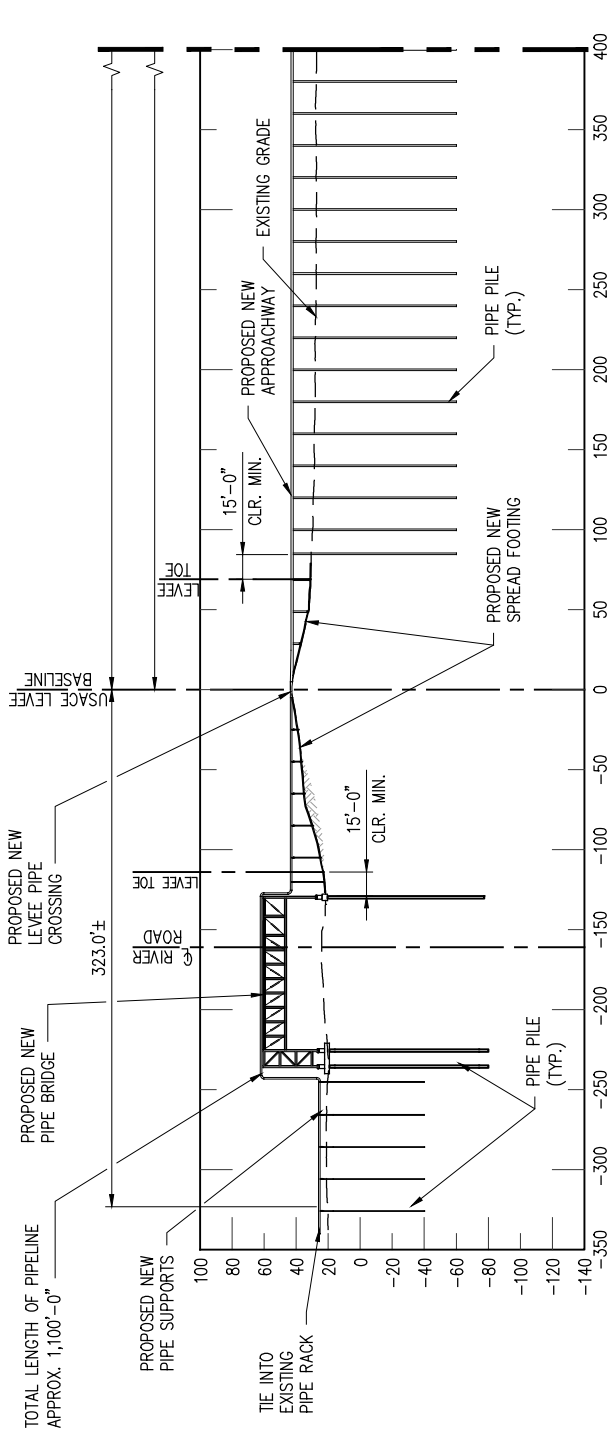
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PROPOSED NEW BARGE DOCK
ENLARGED PROPOSED SITE PLAN

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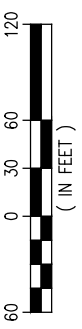
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SECTION @ STA. 1396+58

SCALE: 1" = 120'



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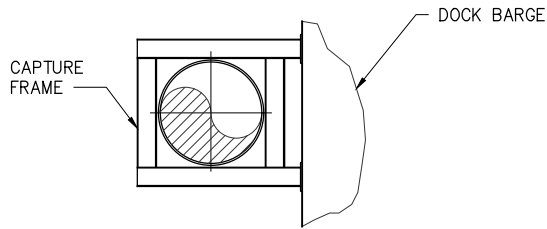
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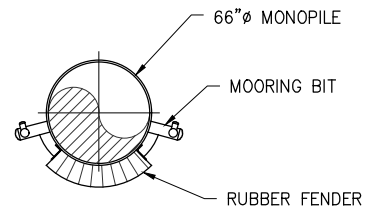
PROPOSED BARGE FLEETING
SECTION @ STA. 1396+58

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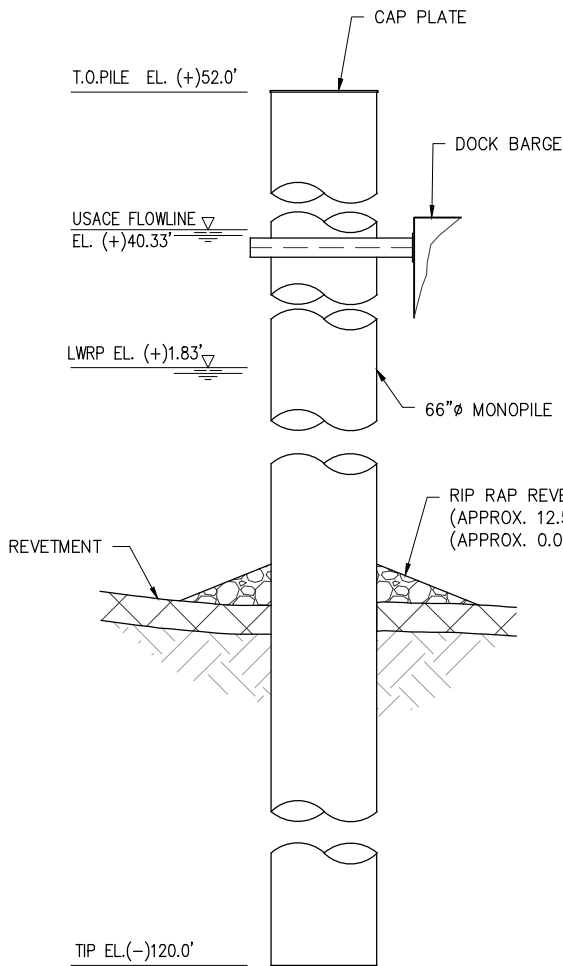
CAPTIVE PILE PLAN

SCALE: 1" = 10'



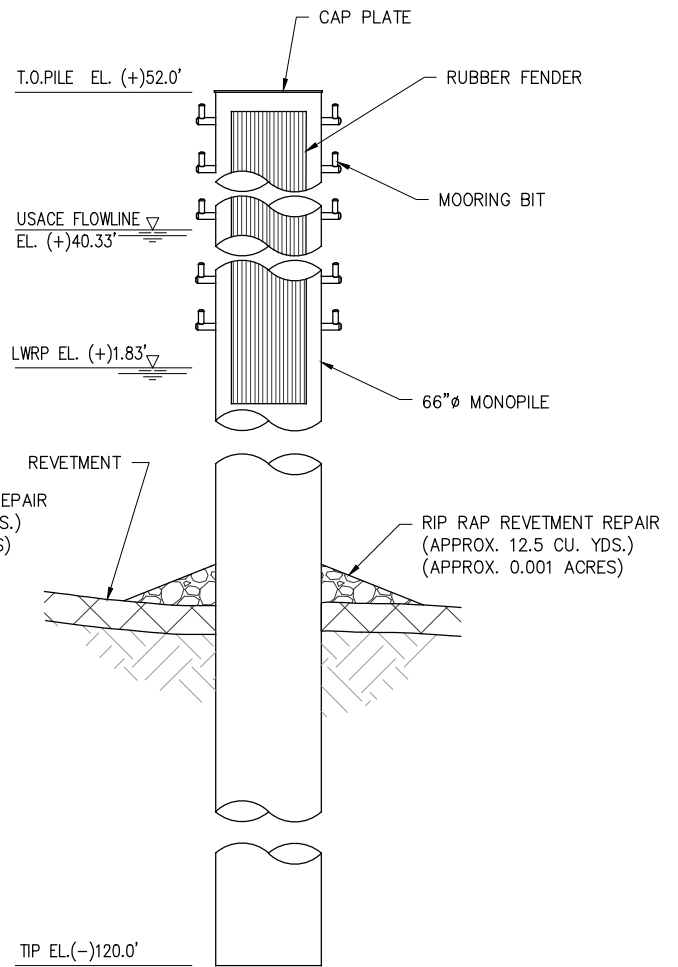
BARGE FENDER PILE PLAN

SCALE: 1" = 10'



CAPTIVE PILE ELEVATION

SCALE: 1" = 10'
(2 REQUIRED)



BARGE FENDER PILE ELEVATION

SCALE: 1" = 10'
(6 REQUIRED)

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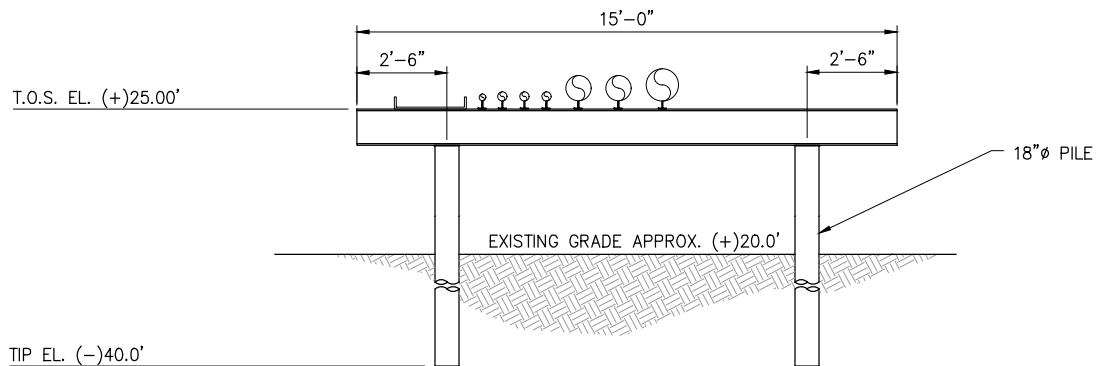
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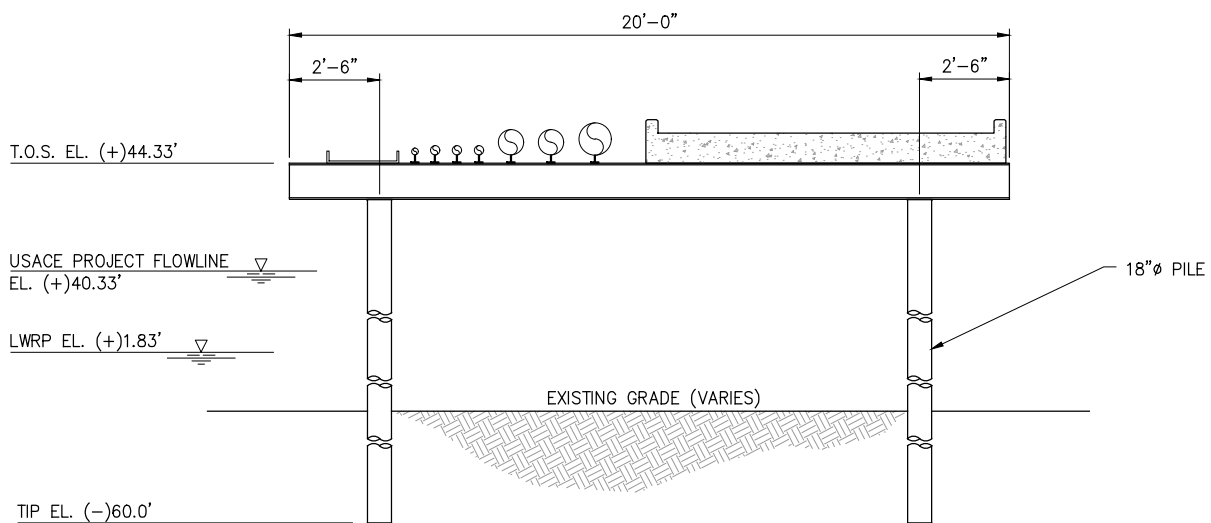
PROPOSED NEW BARGE DOCK
DOCK FENDER PILE DETAILS

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SHEET No.

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TYPICAL PIPE SUPPORT SECTION A
 SCALE: 3/16" = 1'-0"



TYPICAL APPROACHWAY SECTION B
 SCALE: 3/16" = 1'-0"

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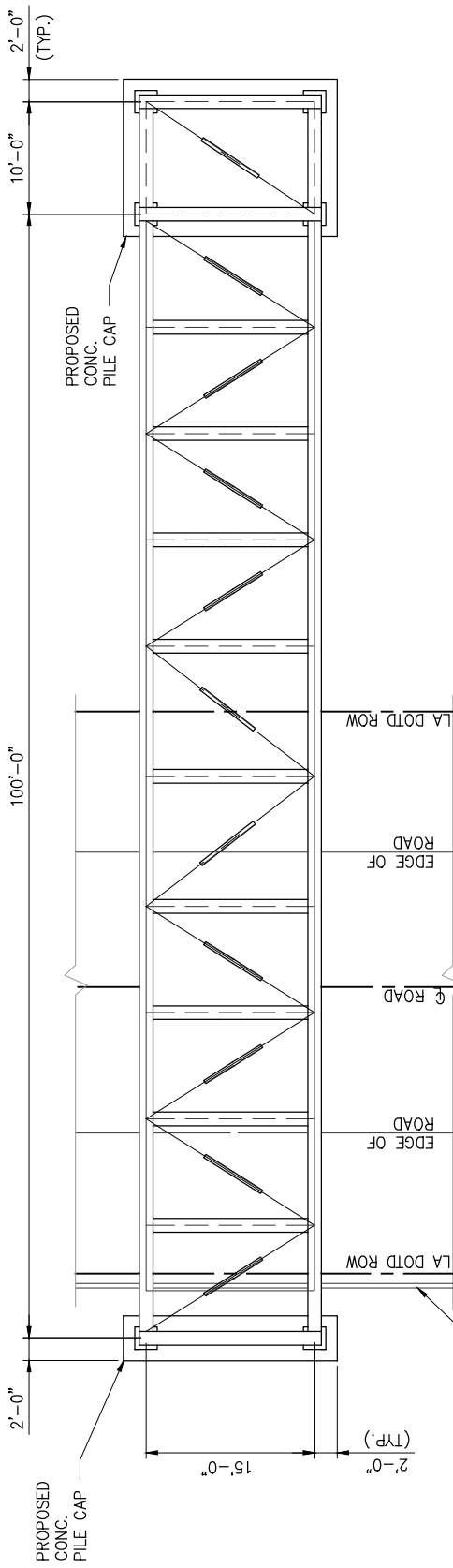
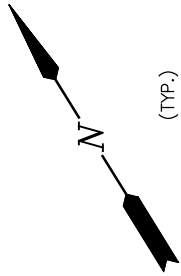
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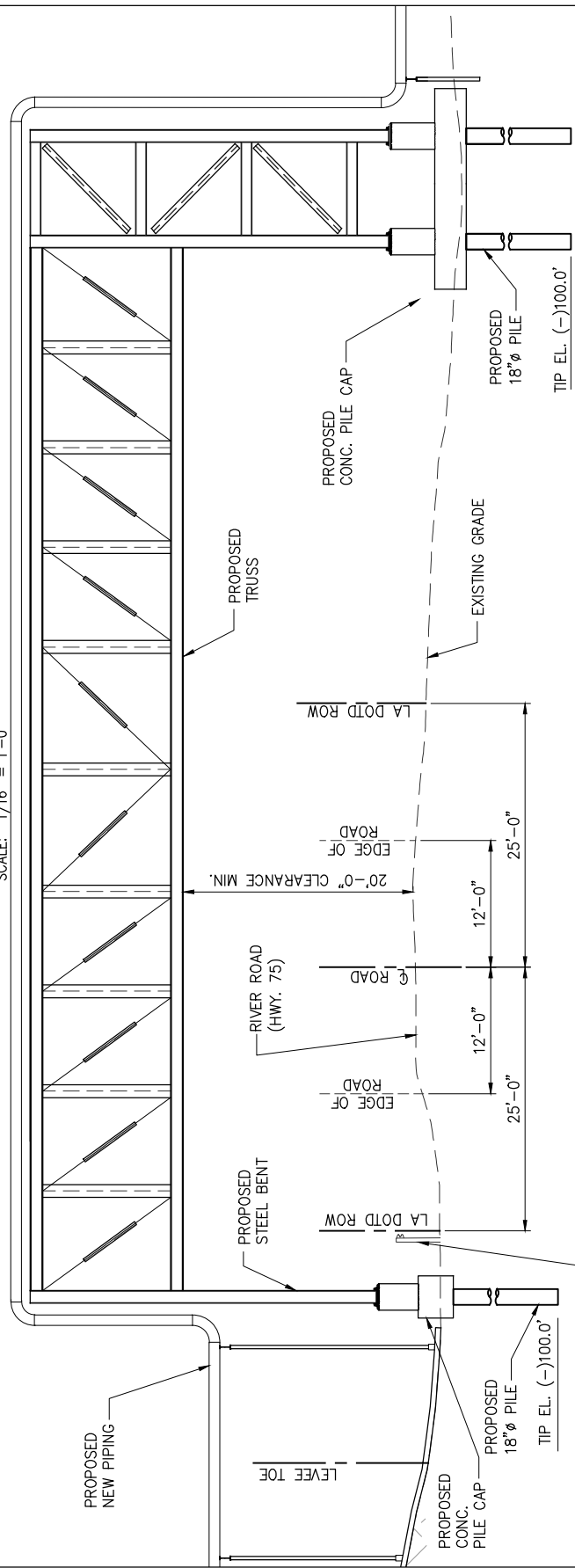
PROPOSED NEW BARGE DOCK
PROPOSED NEW PIPE SUPPORT SECTIONS

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PROPOSED NEW PIPE BRIDGE – PLAN

SCALE: 1/16" = 1'-0"



PROPOSED NEW PIPE BRIDGE – ELEVATION

SCALE: 1/16" = 1'-0"

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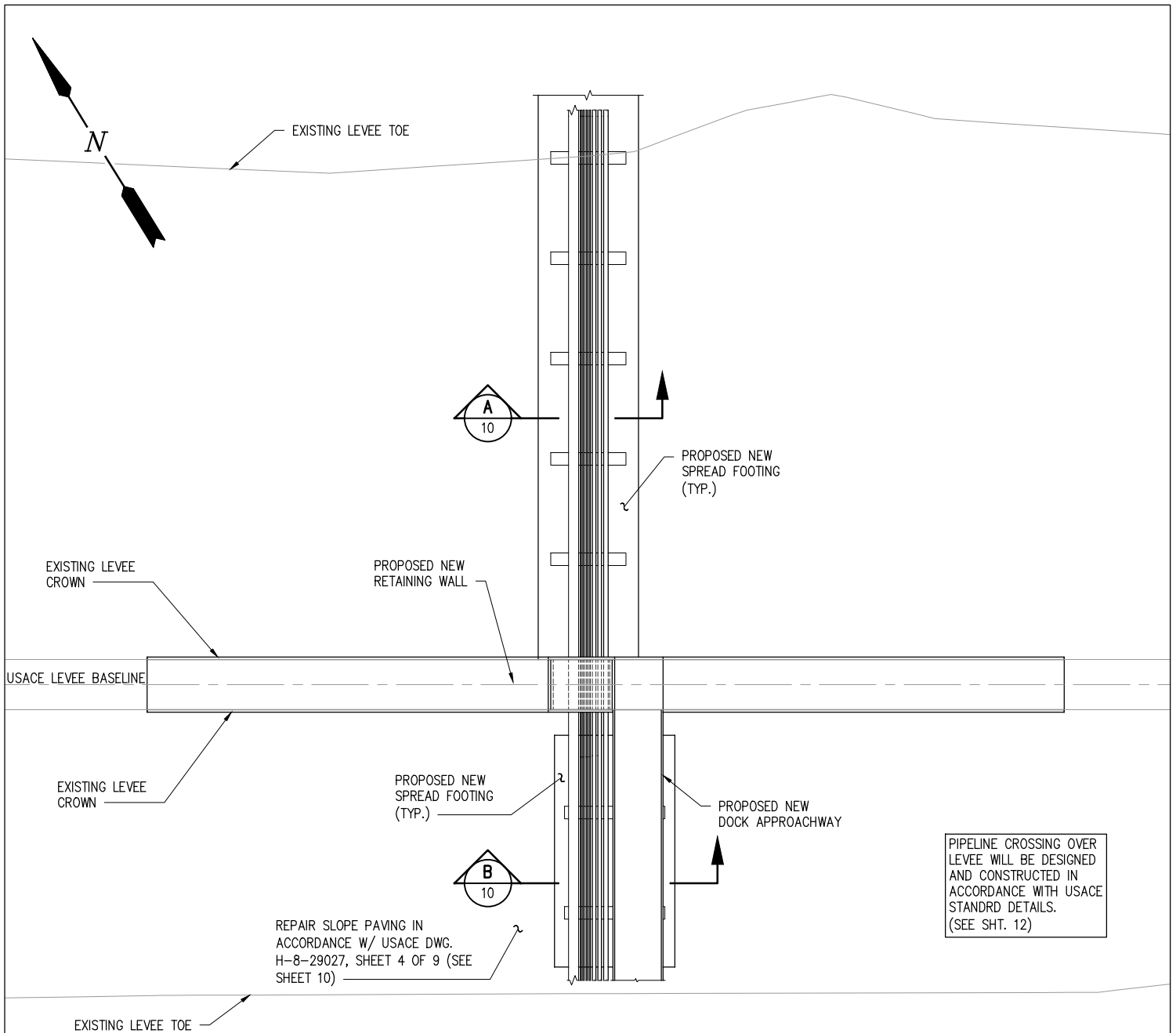
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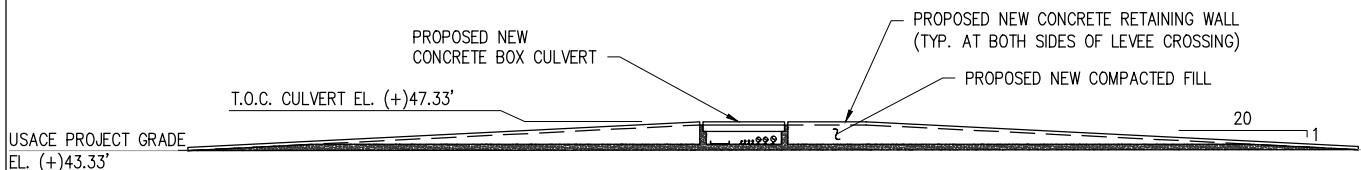
PROPOSED NEW BARGE DOCK
PROPOSED NEW TRUSS
PLAN AND SECTION

DATE JUN. '22
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JOB NO 11583-6
SHEET No.
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PROPOSED NEW LEVEE PIPE CROSSING – PLAN

SCALE: 1" = 30'



PROPOSED NEW LEVEE PIPE CROSSING – ELEVATION

SCALE: 1" = 30'

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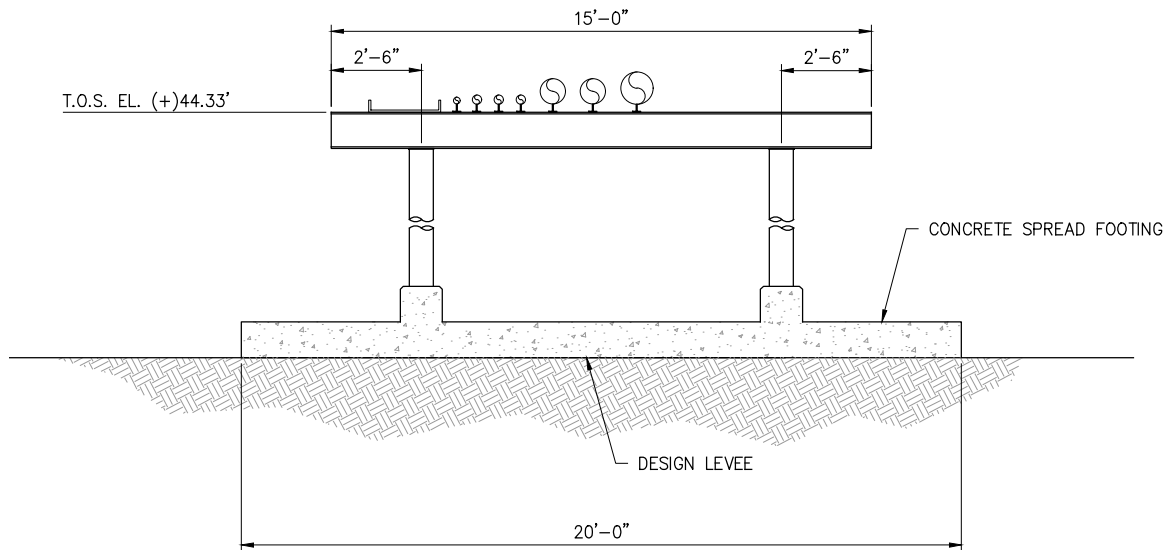
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PROPOSED NEW BARGE DOCK
PROPOSED NEW CULVERT
PLAN AND ELEVATION

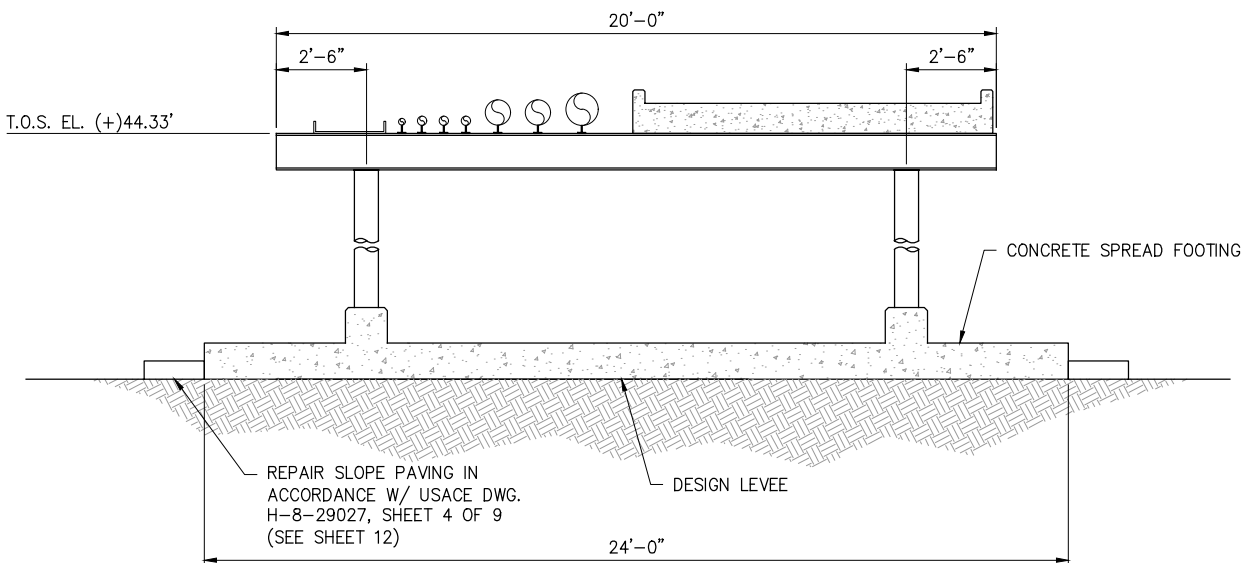
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TYPICAL SPREAD FOOTING LEVEE SECTION

SCALE: 3/16" = 1'-0"

A
9



TYPICAL SPREAD FOOTING LEVEE SECTION

SCALE: 3/16" = 1'-0"

B
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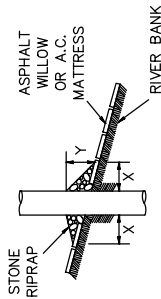
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PROPOSED NEW BARGE DOCK
PROPOSED NEW CROSS SECTIONS

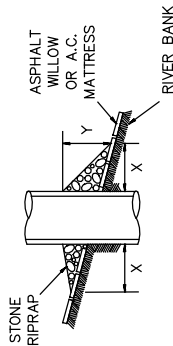
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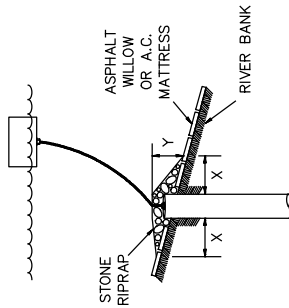
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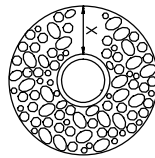
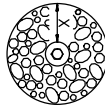
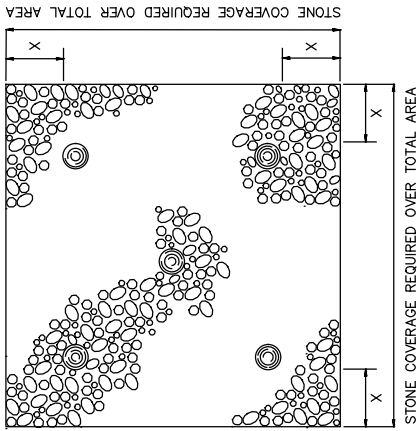
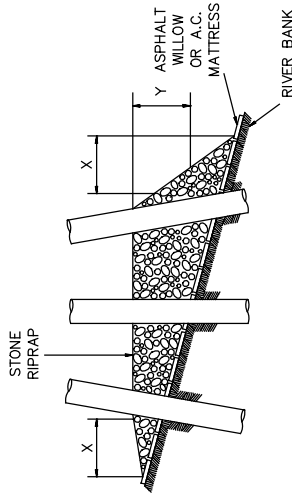
CAISSON



SPUD PILE
(BUOY AND ANCHOR CHAIN)



PILE CLUSTER



REFERENCE USAGE DRAWING:
PIPELINE CROSSINGS OVER LEVEES AND FLOODWALLS
SURFACE CROSSINGS TYPICAL FOR RIVER LEVEES
FILE NUMBER H-8-29027, DRAWING 4 OF 9

NOTES:

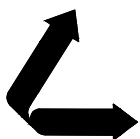
1. SIZE OF RIPRAP TO VARY BETWEEN 6 POUNDS AND 125 POUNDS WITH 40 PERCENT TO 60 PERCENT OF THE STONE WITHIN THE RANGE OF 25 POUNDS TO 75 POUNDS.
2. WHEN PENETRATING THE UPPER BANK PAVING IN A RETEVMENT AREA WITH PILES, CAISSONS AND/OR PILE CLUSTERS, A 10 INCH THICK RIPRAP STONE LAYER SHALL BE PLACED OVER ALL AREAS WHERE THE BANK PAVING IS DISTURBED BY DRIVING OPERATIONS.
3. WHEN USING AN ANCHOR CHAIN AND BUOY SYSTEM, THE ANCHOR CHAIN MUST BE ATTACHED AT THE TOP OF THE PILE TO MINIMIZE RETEVMENT DAMAGE.

DEPTH OF WATER IN FEET AT POINT OF PENETRATION AT TIME OF STONE PLACEMENT	DIMENSIONS IN FEET FOR PROTECTION AREA REQUIRED									
	PILE		CAISSON		SPUD PILE (BUOY AND ANCHOR CHAIN)		PILE CLUSTER			
	X	Y	X	Y	X	Y	X	Y		
1' - 10'	3	1.5	5	2			3	1		
11' - 40'	5	2	8	3	7	3	6	2		
41' - 60'	8	3	12	4	12	4	10	3		

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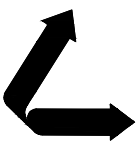
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PROPOSED NEW BARGE DOCK
USAGE RETEVMENT REPAIR DETAILS

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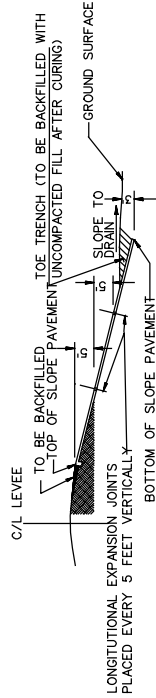
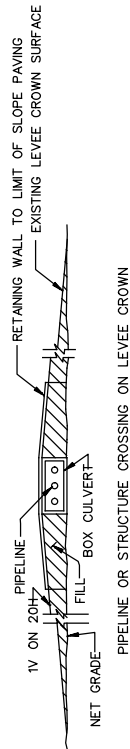
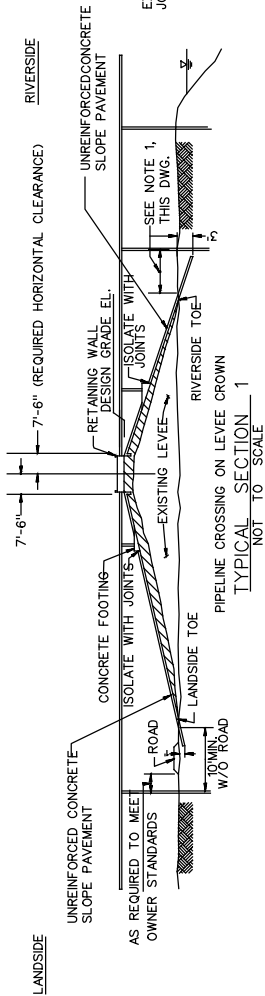
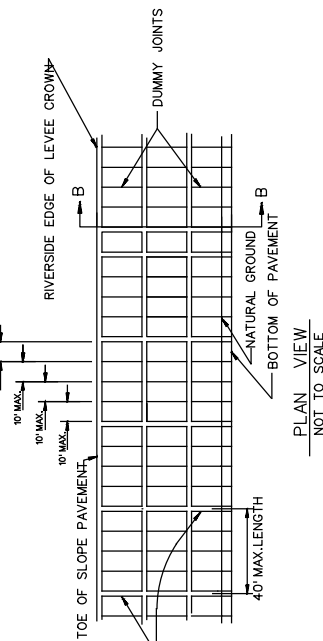
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WILLOW GLEN TERMINAL, LLC.
ST. GABRIEL LOUISIANA

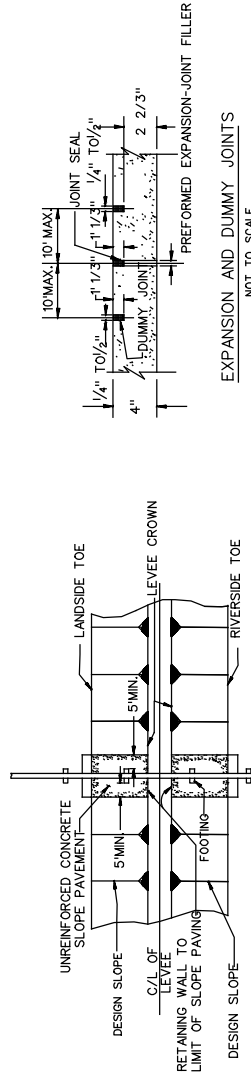
**PROPOSED NEW BARGE DOCK
PIPELINE CROSSINGS OVER LEVEES
AND FLOODWALLS**

DATE JUN. '22
DESIGN CAS
DRAWN ADG
CHECK JDM
JOB NO 11583-6
SHEET No.
12 OF 14

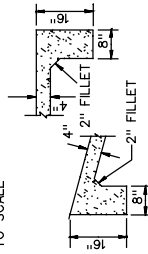
REV
A



HORIZONTAL EXPANSION JOINT
SECTION B - B
NOT TO SCALE



EXPANSION AND DUMMY JOINTS
NOT TO SCALE



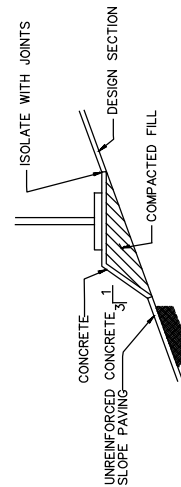
DETAILS OF
CUTOFF WALL
NOT TO SCALE

EMBANKMENT REQUIREMENTS

COMPACTED FILL: (LEVEE ACCESS ROADS AND FOOTINGS)
THE MATERIAL FOR COMPACTED FILL SHALL BE PLACED IN LAYERS, THE FIRST LAYER NOT MORE THAN 4 INCHES IN THICKNESS AND THE SUCCEEDING LAYERS NOT MORE THAN 8 INCHES IN THICKNESS PRIOR TO COMPACTION. EACH LAYER SHALL BE COMPACTED TO A MINIMUM OF 95% RELATIVE COMPACTION. THE FILL SHALL BE PLACED AT A SPEEDS NOT TO EXCEED 3.5 MILES PER HOUR. IF AN ALTERNATE TYPE COMPACTING MACHINE IS USED, TYPE SHOULD BE STATED ON APPLICANT'S DRAWING FOR REVIEW AND APPROVAL.

SEMICOMPACTED FILL (LEVEE)
THE MATERIAL FOR SEMICOMPACTED FILL SHALL BE PLACED OR SPREAD IN LAYERS, THE FIRST LAYER NOT MORE THAN 4 INCHES IN THICKNESS AND THE SUCCEEDING LAYERS NOT MORE THAN 8 INCHES IN THICKNESS PRIOR TO COMPACTION. EACH LAYER SHALL BE COMPACTED BY THREE COMPLETE PASSES OF A CRAWLER-TYPE TRACTOR WEIGHING NOT LESS THAN 20,000 POUNDS AND EXERTING A UNIT TREAD PRESSURE OF NOT LESS THAN 20,000 POUNDS PER SQUARE INCH. THE TRACTOR SHALL BE OPERATED AT SPEEDS NOT TO EXCEED 3.5 MILES PER HOUR. IF AN ALTERNATE TYPE COMPACTING MACHINE IS USED, TYPE SHOULD BE STATED ON APPLICANT'S DRAWING FOR REVIEW AND APPROVAL.

UNCOMPACTED FILL (BERMS AND BACKFILL)
THE MATERIAL FOR UNCOMPACTED FILL SHALL BE PLACED IN APPROXIMATELY HORIZONTAL LAYERS NOT EXCEEDING 3 FEET IN THICKNESS. THE LAYERS SHALL BE UNIFORMLY SPREAD, DISTRIBUTED, AND OTHERWISE MANIPULATED DURING PLACEMENT TO PREVENT THE FORMATION OF LAYERS OR Voids. THE FILL SHALL BE ELIMINATED. THE FILL WILL NOT REMAIN INTACT, AND LARGE, OPEN VOIDS IN THE FILL WILL BE ELIMINATED.



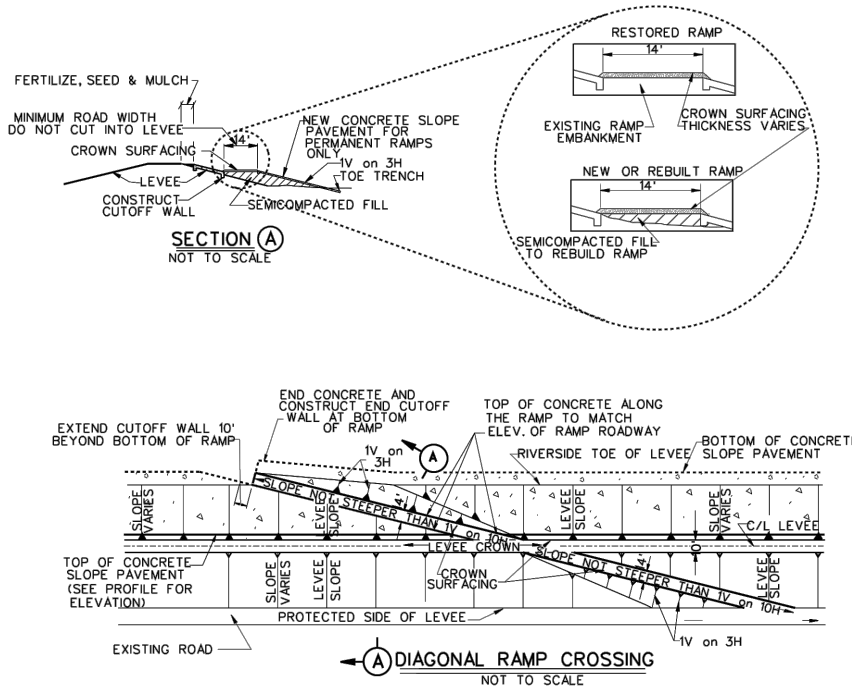
ALTERNATE FOOTING FOR
SLOPES FLATTER THAN 1V ON 3H
DETAIL
NOT TO SCALE

REFERENCE USAGE DRAWING:
PIPELINE CROSSINGS OVER LEVEES AND FLOODWALLS
SURFACE CROSSINGS TYPICAL FOR RIVER LEVEES
FILE NUMBER H-8-29027, DRAWING 4 OF 9

SLOPE PAVEMENT DETAILS

NOTES:

1. 10' MIN. OR AS REQUIRED TO AVOID PENETRATION OF CONCRETE SLOPE PAVEMENT.
2. CONCRETE SLOPE PAVEMENT IS REQUIRED WHEN FOOTING REST ON LEVEE SLOPE PAVEMENT MUST EXTEND 5' EACH SIDE OF FOOTINGS.
3. IN THE ABSENCE OF FOOTING ON LEVEE LANDSIDE SLOPE PAVING WILL NOT BE REQUIRED. RIVERSIDE SLOPING WILL BE REQUIRED WHEN NECESSARY FOR EROSION CONTROL.
4. SMOOTH TRANSITIONS SHALL BE CONSTRUCTED BETWEEN THE LEVEE ENLARGEMENT AND EXISTING LEVEE.
5. ALL FRESH FILL SHALL BE SODDED OR FERTILIZED AND SEEDS AND SHALL BE MAINTAINED UNTIL A HEALTHY GROWTH IS OBTAINED.
6. THE CROWN OF THE ENLARGED LEVEE SHALL BE SURFACED WITH UNCOMPACTED FILL (BERMS AND BACKFILL) TO A MINIMUM OF NINE INCHES IN THICKNESS FOR FULL WIDTH (10' MIN.) AND LENGTH OF RAMP.
7. FILL MATERIAL USED IN CONSTRUCTION OF THE LEVEE ENLARGEMENT, RAMP, PIPE COVER, AND BACKFILL OF EXCAVATION SHALL BE IMPERVIOUS EARTH FILL.
8. PIPELINE MARKERS SHALL BE PLACED AND MAINTAINED AT EACH TOE OF LEVEE IN LINE WITH PIPE CROSSING AND INDICATING DIRECTION AND NUMBER OF LINES, PRODUCT AND ADDRESS FOR CONTACTING OWNER.



REFERENCE USACE DRAWING:
PIPELINE CROSSINGS OVER LEVEES AND FLOODWALLS
SURFACE CROSSINGS TYPICAL FOR RIVER LEVEES
FILE NUMBER H-8-29027, DRAWING 6 OF 9

EMBANKMENT REQUIREMENTS

COMPACTED FILL: (LEVEE ACCESS ROADS AND FOOTINGS)
THE MATERIAL FOR COMPACTED FILL SHALL BE PLACED OR SPREAD IN LAYERS. THE FIRST OR BOTTOM LAYER AND THE LAST TWO LAYERS NOT MORE THAN 6 INCHES IN THICKNESS AND ALL LAYERS BETWEEN THE FIRST AND THE LAST TWO LAYERS NOT MORE THAN 12 INCHES IN THICKNESS PRIOR TO COMPACTION. THE FIRST AND EACH SUCCESSIVE LAYER OF COMPACTED FILL MATERIAL SHALL BE COMPACTED TO AT LEAST 90 PERCENT OF MAXIMUM DRY DENSITY AS DETERMINED BY ASTM D 698 (STANDARD PROCTOR DENSITY) AT A MOISTURE CONTENT WITHIN THE LIMITS OF PLUS 5 TO MINUS 3 PERCENT OF OPTIMUM MOISTURE CONTENT DETERMINED FROM THE STANDARD PROCTOR DENSITY TEST ASTM D 698.

UNCOMPACTED FILL: (BERMS AND BACKFILL)
THE MATERIAL FOR UNCOMPACTED FILL SHALL BE PLACED IN APPROXIMATELY HORIZONTAL LAYERS NOT EXCEEDING 3 FEET IN THICKNESS. THE LAYERS SHALL BE UNIFORMLY SPREAD, DISTRIBUTED, AND OTHERWISE MANIPULATED DURING PLACEMENT TO SUCH AN EXTENT THAT INDIVIDUAL LOADS OF MATERIAL DEPOSITED ON THE FILL WILL NOT REMAIN INTACT, AND LARGE, OPEN VOIDS IN THE FILL WILL BE ELIMINATED.

NOTES:

- 10' MIN. DISTANCE (OR AS OTHERWISE REQUIRED) BETWEEN THE LEVEE TOE AND THE PIPELINE SUPPORT IS REQUIRED TO AVOID PENETRATION OF CONCRETE SLOPE PAVEMENT.
- CONCRETE SLOPE PAVEMENT IS REQUIRED WHEN FOOTINGS REST ON LEVEE SLOPE. PAVEMENT MUST EXTEND 5' EACH SIDE OF FOOTINGS.
- IN THE ABSENCE OF FOOTING ON LEVEE, LANDSIDE SLOPE PAVING WILL NOT BE REQUIRED. RIVERSIDE SLOPING WILL BE REQUIRED WHEN NECESSARY FOR EROSION CONTROL.
- SMOOTH TRANSITIONS SHALL BE CONSTRUCTED BETWEEN THE LEVEE ENLARGEMENT AND EXISTING LEVEE.
- ALL FRESH FILL SHALL BE SODDED OR FERTILIZED AND SEEDS AND SHALL BE MAINTAINED UNTIL HEALTHY GROWTH IS OBTAINED.
- THE CROWN OF THE ENLARGED LEVEE AND THE LEVEE ACCESS RAMPS SHALL BE SURFACED WITH CRUSHED STONE (7" IN THICKNESS [LOOSE MEASUREMENT] FOR EXISTING CROWN AND RAMPS AND 9" FOR NEW CONSTRUCTION) FOR THE FULL WIDTH (10' MIN.) AND LENGTH OF THE ENLARGED LEVEE OR RAMP. THE CRUSHED STONE SURFACING SHALL MEET THE REQUIREMENTS OF LSSRB SECTION 1003.04 (a), 2000 EDITION.
- FILL MATERIAL USED IN CONSTRUCTION OF THE LEVEE ENLARGEMENT, RAMPS, PIPE COVER, AND BACKFILL OF EXCAVATION SHALL BE IMPERVIOUS EARTH FILL.
- A PIPELINE MARKER SHALL BE PLACED AND MAINTAINED AT EACH LEVEE TOE IN LINE WITH THE PIPELINE CROSSING AND INDICATE OWNER, SIZE, NUMBER OF LINES, PRODUCT, AND ADDRESS FOR CONTACTING OWNER.

BRADFORD N. FRISCHERTZ, P.E.
LA PE #32628

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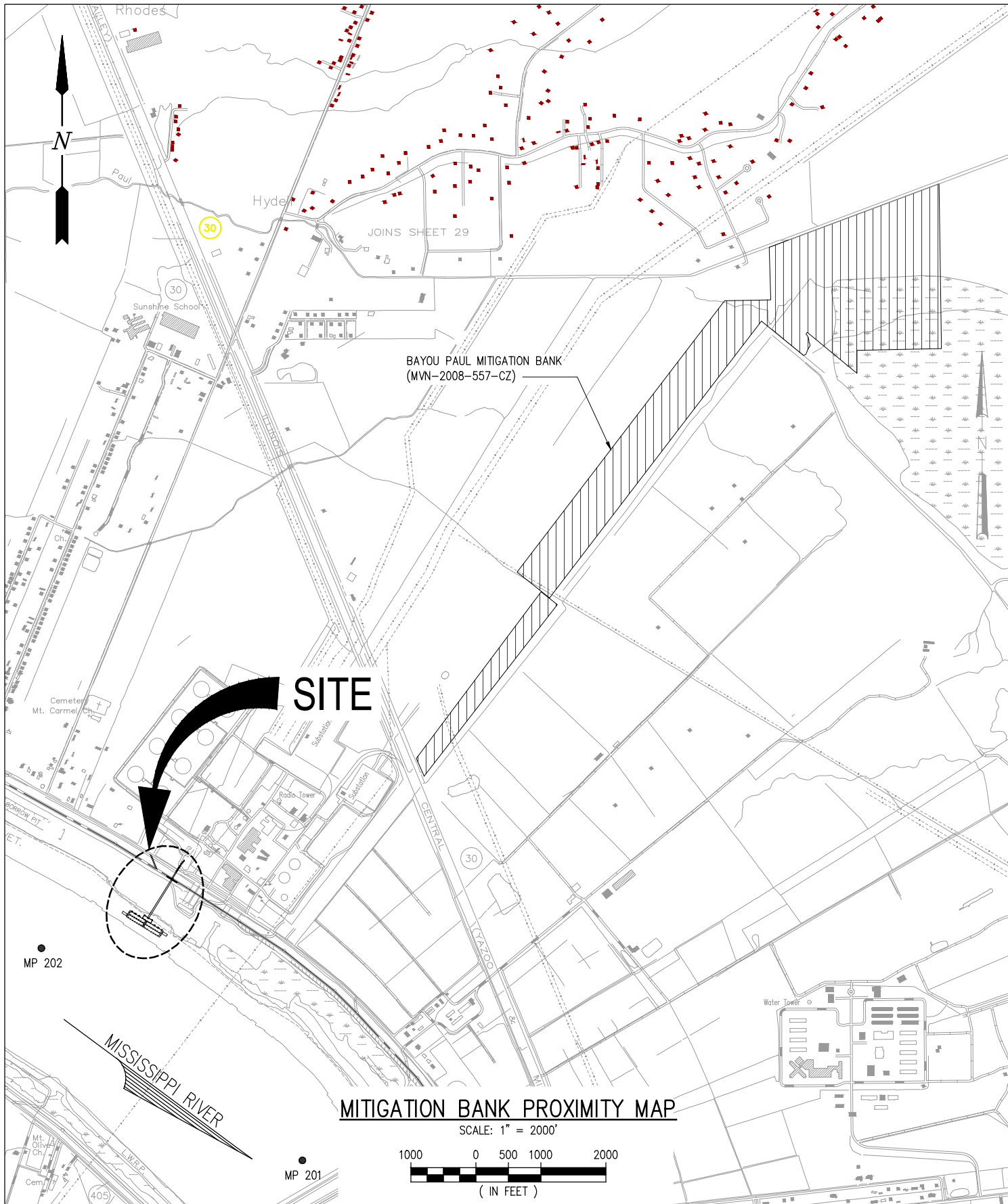
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CONSULTING ENGINEERS
INCORPORATED

LA: EF-1120 TX: F-2981
NEW ORLEANS • BEAUMONT • CORPUS CHRISTI • HOUSTON

WILLOW GLEN TERMINAL, LLC.
ST. GABRIEL LOUISIANA

PROPOSED NEW BARGE DOCK
USACE LEVEE RAMP CROSSING DETAILS

DATE JUNE '22
DESIGN BNF
DRAWN ADG
CHECK JDM
JOB NO 11583-6
SHEET No.
13 OF 14



BRADFORD N. FRISCHERTZ, P.E.
LA PE #32628

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ST. GABRIEL LOUISIANA

PROPOSED NEW BARGE DOCK
MITIGATION BANK PROXIMITY MAP

DATE JUNE '22
DESIGN BNF
DRAWN ADG
CHECK JDM
JOB NO 11583-6
SHEET No.
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