



REPLY TO
ATTENTION OF

DEPARTMENT OF THE ARMY
CORPS OF ENGINEERS, NEW ORLEANS DISTRICT
7400 LEAKE AVENUE
NEW ORLEANS, LOUISIANA 70118

Regional Planning and
Environmental Division South
Environmental Compliance Branch

October 31, 2017

PUBLIC NOTICE
REQUEST FOR PERMISSION TO ALTER A U.S. ARMY CORPS OF ENGINEERS
PROJECT PURSUANT TO 33 U.S.C. SECTION 408

PUBLIC NOTICE/FILE NUMBER: 17-0244, Illinois Central Railroad (ICRR) proposes to replace their rail bridge across the Bonnet Carre Spillway near Norco, St. Charles Parish, Louisiana.

Interested parties are hereby notified that a request for permission to alter a U.S. Army Corps of Engineers (USACE) project pursuant to Section 14 of the Rivers and Harbors Act of 1899 (33 U.S.C. Section 408) has been received and will be evaluated by the New Orleans District. Written comments, including any objections to the proposed alteration, stating reasons therefor, are being solicited from anyone having an interest in the requested alteration. Comments must be mailed or emailed by or before the last day of the comment period and must reference the file number of this public notice. Comments will become part of the record and will be considered in the decision on the request. See below for contact information and address. The proposed action could alter the Bonnet Carre Spillway federal project.

COMMENT PERIOD EXPIRES: November 15, 2017

REQUESTED ALTERATION: ICRR proposes to replace an existing timber and concrete railroad bridge with a new concrete railroad bridge across the Bonnet Carre Spillway near the southwest shore of Lake Pontchartrain. The bridge would be approximately 11,000-feet long and would be constructed 50 feet north of the existing timber trestle bridge with tie-backs into the existing rail alignment near both the upper and lower guide levees of the spillway. The new bridge would be constructed by track-mounted equipment on the lake side of the existing bridge. Temporary roads will be constructed with timber mats to the south of the existing railroad alignment to access the equipment staging areas that will be located near the upper and lower guide levees. The existing timber bridge would be dismantled and removed following construction of the new bridge.

The attached figures provide the location and details of the proposed project.

REQUESTER: Illinois Central Railroad, c/o HDR Engineering, 1 Galleria Blvd., Suite 1920, Metairie, LA 70001

PERMISSION AUTHORITY: The authority to grant permission for temporary or permanent use, occupation or alteration of any USACE civil works project is contained in Section 14 of the Rivers and Harbors Act of 1899, as amended, codified at 33 USC 408 ("Section 408"). Section 408 authorizes the Secretary of the Army to grant permission for the alteration or occupation or use of a USACE project if the Secretary determines that the activity will not be injurious to the public interest and will not impair the usefulness of the project.

ADDITIONAL INFORMATION: ICRR has submitted an application for a Rivers and Harbors Act Section 10/404 permit to the USACE Regulatory Branch for the proposed action. That evaluation (MVN-2015-02351-2-ES) will be a separate process from the evaluation of the request for permission to alter a USACE project. This Public Notice solicits comments only on the proposed alteration of the USACE project.

Materials for the proposed project would be delivered to and hauled from the project site via Prescott Road which is located to the northwest of the upper guide levee and by the lower guide levee road at the southeast limit of the project.

LOCATION OF PROPOSED ACTION: Along the southwest shore of Lake Pontchartrain, parallel to and south of Interstate Highway 10, crossing the Bonnet Carre Spillway in St. Charles Parish, near Latitude: 30°04'2.86" and Longitude: -90°23'31.20"W as shown on the attached drawings.

WRITTEN COMMENTS MAY BE MAILED TO:

Department of the Army
Corps of Engineers, New Orleans District
CEMVN-PDC
ATTN: Joseph Musso
7400 Leake Ave.
New Orleans, Louisiana 70118

WRITTEN COMMENTS MAY BE EMAILED TO: joseph.r.musso@usace.army.mil

ALL comments must contain the full name and address of the individual commenting and must be received by the USACE by the expiration of the public notice period.

EVALUATION: This request will be reviewed pursuant to Section 408 and USACE Engineering Circular (EC) 1165-2-216, which provides policy and procedural guidance for processing requests to alter USACE civil works projects.

The decision whether to grant permission for the requested alteration will be based on

several factors. The benefits that reasonably may be expected to accrue from the proposal will be balanced against its reasonably foreseeable detriments. Review of the requests for modification will be reviewed by a USACE technical review team considering the following factors:

- 1) Potential to Impair the Usefulness of the Project. Proposed alterations will be reviewed to determine whether the alteration would limit the ability of the USACE project to function as authorized, or would compromise or change any authorized project conditions, purposes or outputs. If USACE determines that the usefulness of the authorized project would be impaired, the request will be denied.
- 2) Potential to be Injurious to the Public Interest. Proposed alterations will be reviewed to determine the probable impacts, including cumulative impacts, on the public interest. Factors that may be relevant to the public interest depend upon the type of USACE project being altered and may include, but are not limited to, such things as conservation, economic development, historic properties, cultural resources, environmental impacts, water supply, water quality, flood hazards, floodplains, residual risk, induced damages, navigation, shore erosion or accretion, and recreation. This evaluation will consider information received from the interested parties, including tribes, agencies, and the public. The decision whether to approve an alteration will be determined by the consideration of whether benefits are commensurate with risks. If the potential detriments are found to outweigh the potential benefits, then it may be determined that the proposed alteration is injurious to the public interest.
- 3) Environmental Compliance. A decision on a Section 408 request is a Federal action and is subject to the National Environmental Policy Act (NEPA) and other environmental compliance requirements. As a bridge over navigable waters of the United States, the United States Coast Guard (USCG) will evaluate the proposed action under Section 9 of the Rivers and Harbors Act of 1899. Subsequently, the USGS, Bridges Section has assumed the role of Lead Federal Agency. USACE is an official cooperating agency with the USCG. An Environmental Assessment (EA) that conforms to NEPA requirements will be completed to evaluate the proposed alteration on important resource. That NEPA document will be used by both the USCG and USACE to support their respective NEPA decision documents. Comments received in response to this public notice will be used in the preparation of the USACE decision document. Any comments regarding the proposed alteration should be submitted during the current public comment period.

While ensuring environmental compliance is the responsibility of USACE, the requester is responsible for providing all information that USACE identifies as necessary. Environmental compliance factors that will be considered in this evaluation include but are not limited to the following:

- a. Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) directs all Federal agencies to insure that any action they authorize, fund, or carry-out does not jeopardize the continued existence of an endangered or threatened species including adverse modification of their critical habitat. The U.S. Fish and Wildlife Service (USFWS) has concurred that a determination of "not likely to adversely affect" West Indian manatee and Atlantic sturgeon is appropriate for the proposed Section 408 action. As specified in the determination, Standard Manatee Conditions for In-Water Activities, would be incorporated into the Section 408 Permission, if granted. Atlantic sturgeon may also be present within the proposed project area, but as per the USFWS concurrence document, no special conditions would be required. The proposed action would have "no effect" on other listed species or designated critical habitats. No further coordination under Section 7 of the ESA is necessary.
- b. The requester will be required to provide proof of compliance with Section 401 of the Clean Water Act from the Department of Environmental Quality.
- c. Compliance with Clean Water Act, Section 404(b)(1) guidelines, as well as Section 10 of the Rivers and Harbors Act will be evaluated through a separate USACE permit review process. EC 1165-2-216 specifies that the Section 408 request must be evaluated independent of the above referenced USACE permit evaluations.
- d. The requester has submitted a Coastal Use Permit Application (P20170045) to the Office of Coastal Zone Management, and must provide proof the proposed action is consistent with the Coastal Zone Management Act prior to construction of the proposed action.
- e. In accordance with Section 106 of the National Historic Preservation Act (NHPA), the USACE is required to take into account the effects of any permitted action to historic properties, which includes both archaeological resources on the surface or below ground, as well as historic buildings and structures that are listed in or are eligible for listing in the National Register of Historic Places. Additionally, the USACE will consult with Indian Tribes in accordance with Section 101(d)(6)(B) of the NHPA, the USACE Tribal Consultation Policy, and Executive Order 13175, "Consultation and Coordination with Indian Tribal Governments."

The current railroad bridge has been determined eligible for the National Register of Historic Places and its demolition is an Adverse Effect. The USACE is acting as the lead federal agency for compliance with Section 106 and will be developing a Memorandum of Agreement to resolve Adverse Effects to this historic property, in coordination with the U.S. Coast Guard, the Louisiana State Historic Preservation Office, Canadian National Railroad, and other consulting parties. It is anticipated that the first

consultation meeting to develop this MOA will be held the week of December 4-8, 2017 at the Bonnet Carre Spillway Offices, 16302 River Road, Norco, LA 70079.

All factors that may be relevant to the proposal will be considered, including the potential cumulative effects associated with the proposed project. The review will consider the potential impact to the usefulness of the Federal project and whether the proposed alteration would be injurious to the public interest. Policy and legal compliance will also be considered.

USACE is soliciting comments from interested parties to evaluate the potential impacts of the proposed alteration. Comments will be considered to assess impacts on the public interest factors listed above, as well as endangered species, historic properties, water quality, and general environmental effects. Comments will be used for environmental compliance coordination as required by NEPA.

COORDINATION: This public notice will be coordinated with the relevant State, Federal and Tribal entities as appropriate.

Issuance of this notice does not imply Corps of Engineers endorsement of the project as described.

If you have any questions, please contact Joseph Musso, by telephone at 504-862-2280 or via email at joseph.r.musso@usace.army.mil.

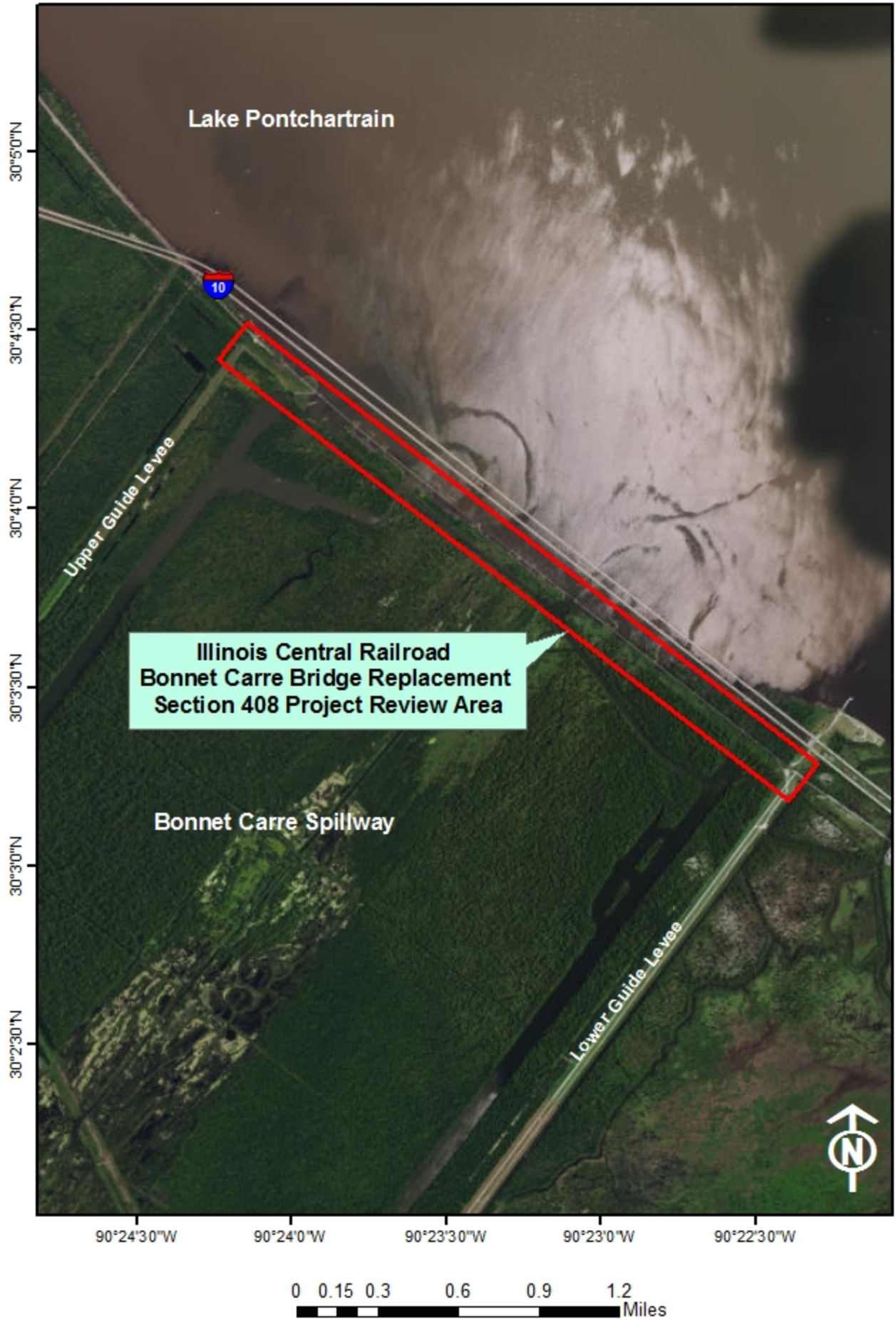
FOR THE DISTRICT COMMANDER:

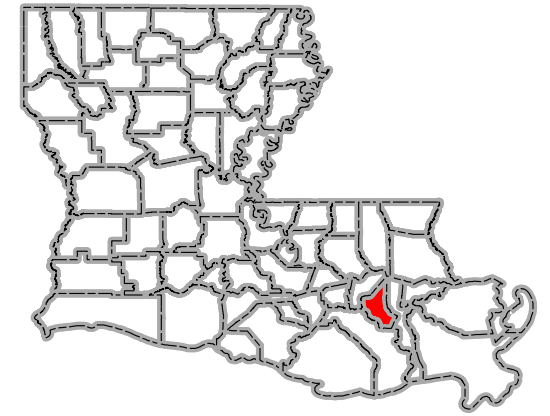







Marshall K. Harper
Chief, Environmental Planning Branch

Enclosures

**Illinois Central Railroad Bonnet Carre Bridge Replacement
Section 408 Area Map
St. Charles Parish, Louisiana**





-  APE
-  PARISH BOUNDARY
-  USGS QUADRANGLE MAP
-  RAILROAD
-  ROADWAY

DATA SOURCES: LA DOTD, ESRI, US CENSUS BUREAU

BONNET CARRE SPILLWAY BRIDGE

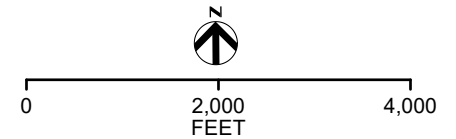
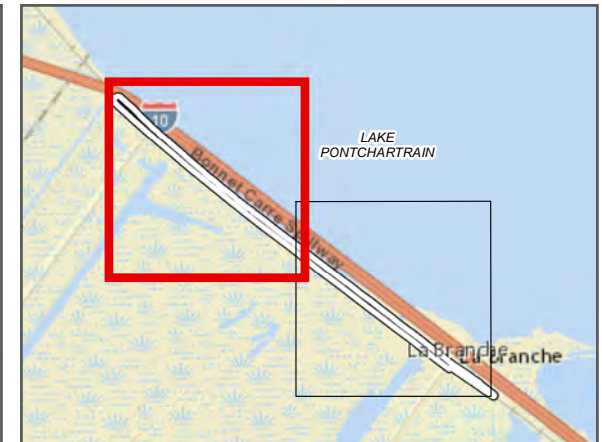


FIGURE 2



- EXISTING RAILROAD CENTERLINE (TO BE REMOVED)
- PROPOSED RAIL CENTERLINE
- ICRR RIGHT-OF-WAY
- EXISTING PRESCOTT ROAD
- CONSTRUCTION ACCESS/STAGING
- TEMPORARY WORKSPACE FOR DEMOLITION
- TREE CLEARING
- EQUIPMENT WORKSPACE
- NEW FILL
- TIMBER MATS (ACCESS)

HDR ENGINEERING, INC.

**ICRR BONNET CARRE
BRIDGE REPLACEMENT**
SECTION 408 ENVIRONMENTAL REVIEW

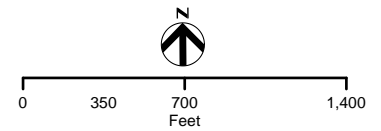
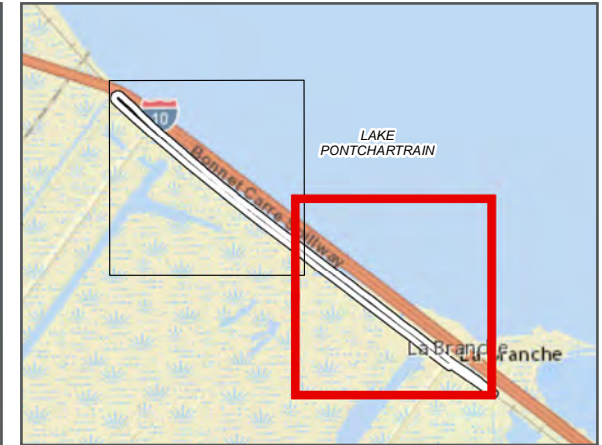
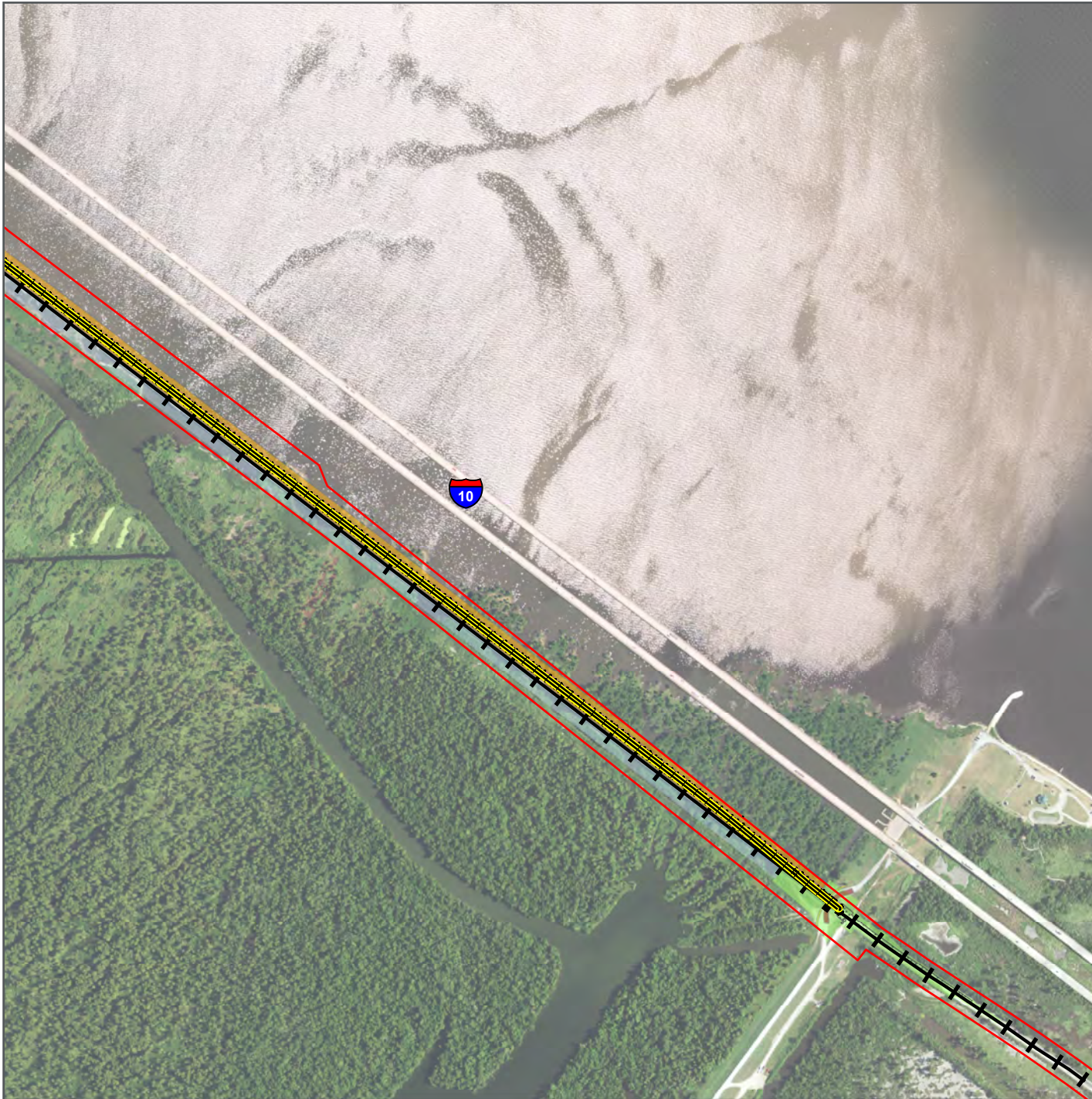


FIGURE 1-1



- ✚ EXISTING RAILROAD CENTERLINE (TO BE REMOVED)
- ▬ PROPOSED RAIL CENTERLINE
- ▭ ICRR RIGHT-OF-WAY
- ▭ CONSTRUCTION ACCESS/STAGING
- ▭ TEMPORARY WORKSPACE FOR DEMOLITION
- ▭ BERM
- ▭ PARISH ROAD
- ▭ TREE CLEARING
- ▭ EQUIPMENT WORKSPACE
- ▭ NEW FILL

HDR ENGINEERING, INC.

**ICRR BONNET CARRE
BRIDGE REPLACEMENT**
SECTION 408 ENVIRONMENTAL REVIEW

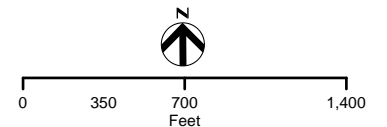
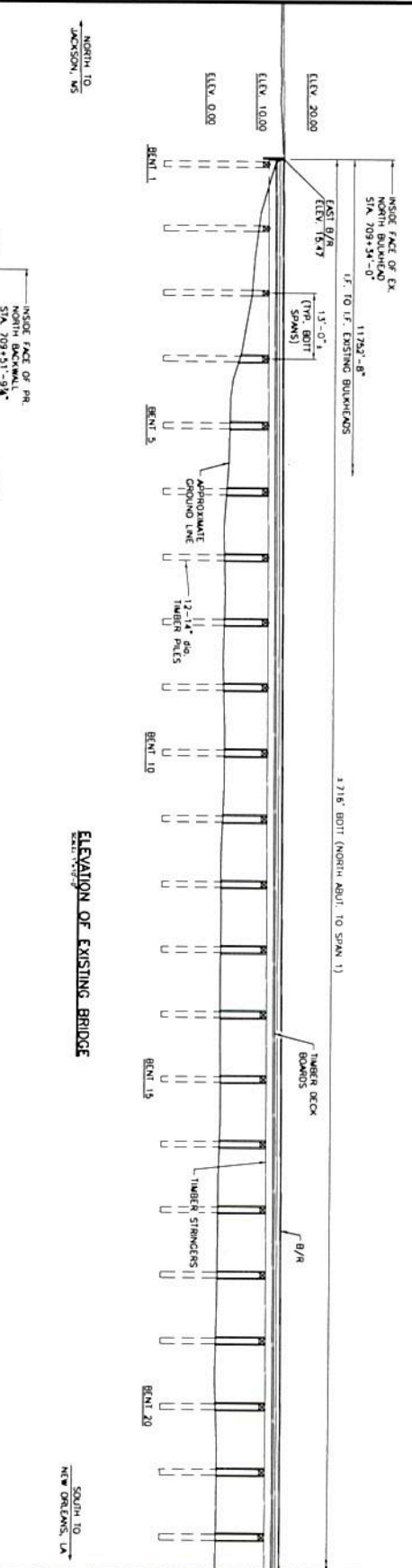
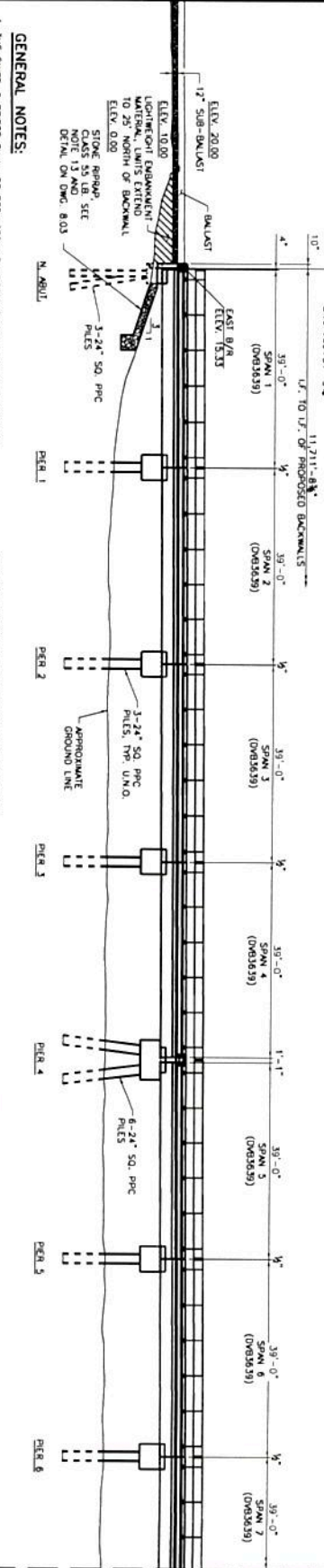


FIGURE 1-2



ELEVATION OF EXISTING BRIDGE



ELEVATION OF PROPOSED BRIDGE

- GENERAL NOTES:**
- THE EXISTING BRIDGE SHALL BE REPLACED WITH PRESTRESSED PRECAST CONCRETE DECK GIRDERS SUPPORTED ON PRECAST CONCRETE PIER AND ABUTMENT CAPS FOUNDED ON PILE CAPS.
 - LENGTH OF EXISTING BRIDGE = 11,232'-8" U.F. TO I.F. EXISTING BALASTINGS.
 - LENGTH OF PROPOSED BRIDGE = 11,211'-8 1/4" U.F. TO I.F. PROPOSED BACKWALLS.
 - HORIZONTAL ALIGNMENT OF TRACK WILL BE SHIFTED APPROXIMATELY 60' EAST FROM AT EACH END OF THE BRIDGE WHERE THE ALIGNMENT WILL TRANSITION BACK CLOSER TO THE EXISTING TRACK IN ORDER TO UTILIZE THE OLD NORTHBOUND MAIN EMBANKMENT. SEE DRAWINGS B.02 AND B.03 FOR HORIZONTAL ALIGNMENT INFORMATION. VERTICAL ALIGNMENT WILL BE AS SHOWN ON DRAWING B.04.
 - DESIGN LIVE LOAD: COOPER E90, IMPACT = 225%/1.
 - WATERPROOFING MEMBRANE SHALL BE INSTALLED OVER THE ENTIRE LENGTH OF LONGITUDINAL JOINTS AND TRANSVERSE JOINTS BETWEEN GIRDERS INCLUDING OVER DECK JOINT COVER PLATES AND UNDERPILING AND DECK ON SPAN B.105.

- BACKFILL BEHIND ABUTMENTS AND WALLS SHALL CONSIST OF FINE GRANULAR MATERIAL COMPACTED TO 98% STANDARD PROCTOR MAXIMUM DENSITY AND 5% COMPACTED IN LIFTS NOT EXCEEDING 12".
- CONSTRUCTION OF THE PROPOSED BRIDGE WILL BE CONSTRUCTED OUTSIDE THE LIFTS OF THE EXISTING BRIDGE WITH THE FOLLOWING EXCEPTIONS:
 - PARTIAL REMOVAL OF FIREWALL #1 WILL BE REQUIRED TO SET ONE SPAN 9.
 - PARTIAL REMOVAL OF TIMBER BENTS (ABANDONED TRACK SIDE ONLY) WILL BE REQUIRED TO SET ONE SPAN NEAR EACH END OF BRIDGE.
- TRAIN TRAFFIC SHALL BE REROUTED ONTO THE PROPOSED BRIDGE AS SOON AS PRACTICABLE AFTER COMPLETION OF THE BRIDGE. AFTER TRAINS ARE RUNNING ON THE NEW BRIDGE AND AS APPROVED BY THE RAILROAD, REMOVAL OF THE EXISTING BRIDGE SHALL BE COMPLETED IN THE ORDER SHOWN ON DRAWING B.01. THE EXISTING GROUND LINE EXCEPT BENTS B20 THROUGH B25 SHALL BE REMOVED TO 3 FEET BELOW GROUND LINE. CONCRETE FIREWALLS AND PIER FOUNDATION SHALL BE REMOVED TO 2 FEET BELOW GROUND LINE. THE SPAN AND BEDROCK SHALL BE REMOVED TO 2 FEET BELOW GROUND LINE. THE SPAN AND BEDROCK SHALL BE REMOVED TO 2 FEET BELOW GROUND EXCEPT EXISTING PILE SPAN 7 AND ITS SUPPORTING PIERS SHALL BE REMOVED.
- NEAREST STATION: FIREWALL, MCGOWAN SUBDIVISION, MILE POST 897.6.
- MILEAGE INCREASES TO THE SOUTH AND IS SHOWN TO THE RIGHT ON THE DRAWINGS.
- INNER GROUNDLINE TO BE INSTALLED PER TRACK STANDARD DRAWING TS-1108 FOR BRIDGE DECK PROFILES.
- STONE GROUP MATERIAL AND PACKING SHALL BE IN ACCORDANCE WITH THE TRACK STANDARD DRAWING TS-1108 FOR BRIDGE DECK PROFILES.
- REPAIR SHALL BE 18" MINIMUM CORRELATIVE FABRIC NOT REQUIRED.

60% PLANS
JUNE 20, 2017
SENIOR STRUCTURE ENGINEER

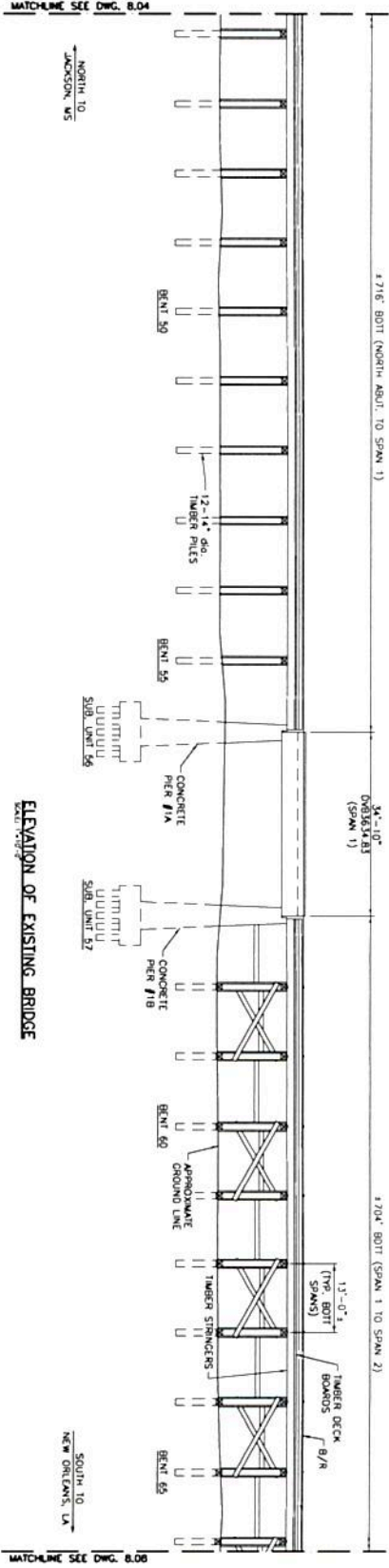
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BRIDGE 891.50
NEW ORLEANS, LA
GENERAL REPLACEMENT
GENERAL LAYOUT (1 OF 78)

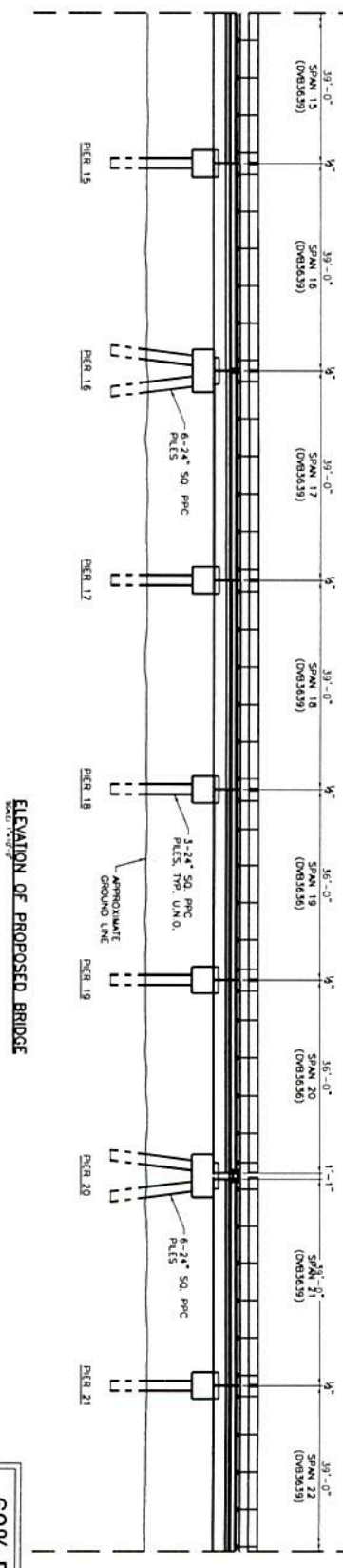
ESCA
Office of Chief Engineer
Bureau de l'ingénieur en chef

EN

12/28/04
Project No. 08150 MCCOAB
Sheet Number 08150-001
Date 01/18/05
Scale 1"=10'-0"



ELEVATION OF EXISTING BRIDGE



ELEVATION OF PROPOSED BRIDGE

60% PLANS
JUNE 20, 2017

SENIOR STRUCTURAL ENGINEER

NO.	DATE	DESCRIPTION	BY	CHKD.
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3		BRIDGE REPLACEMENT	MS	MS
4		GENERAL LAYOUT (5 OF 78)	MS	MS

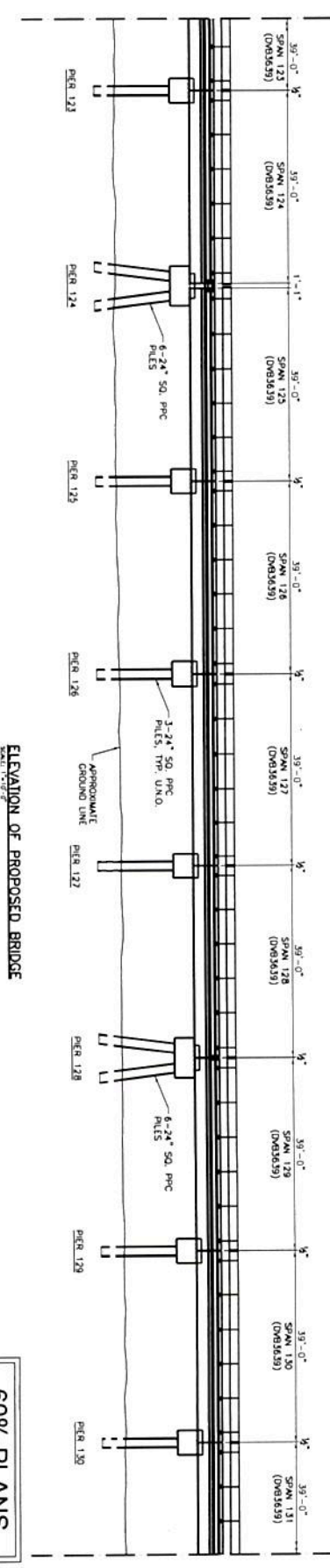
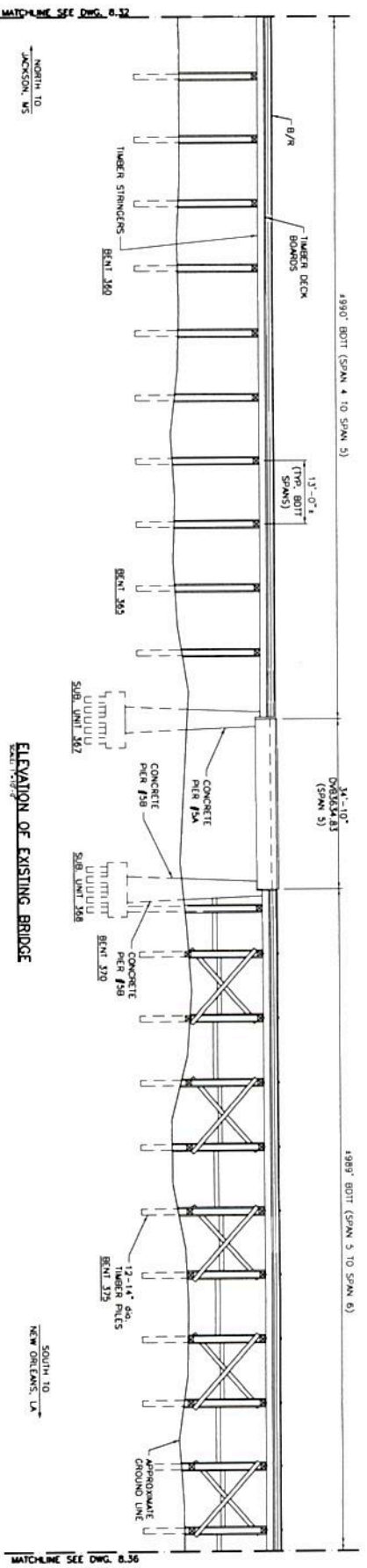


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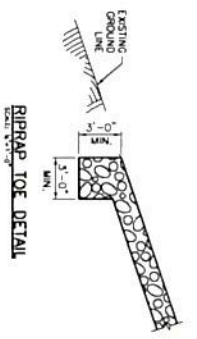
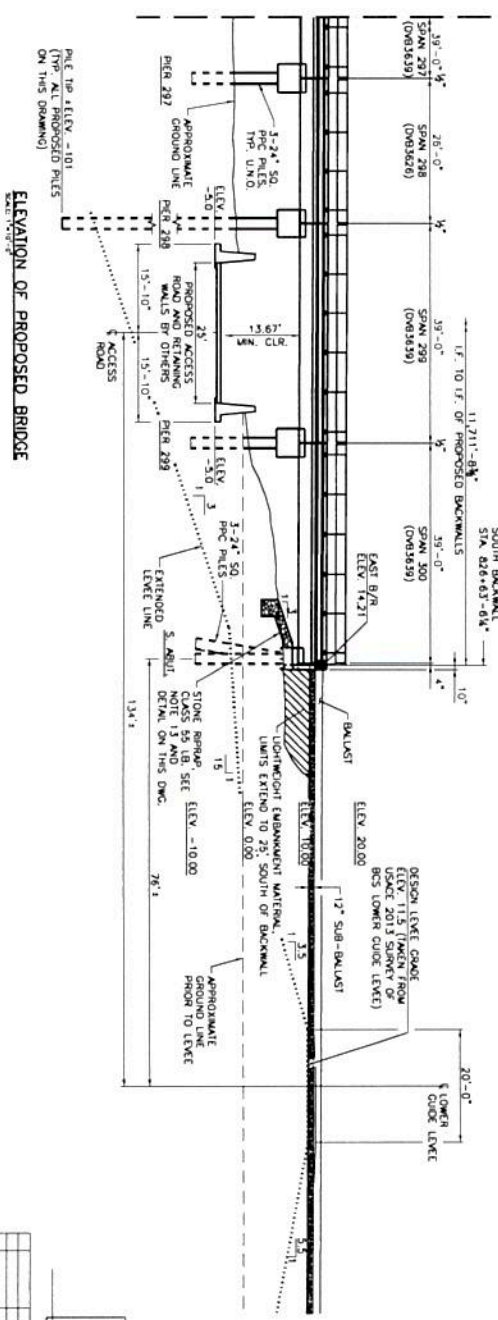
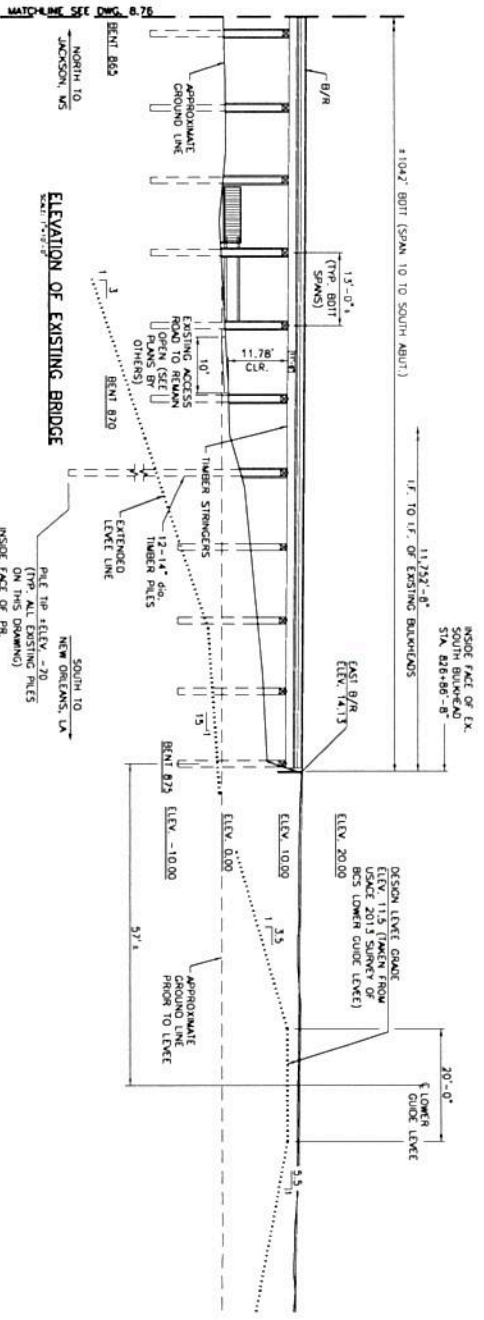
SCALE: 1"=10'-0"



60% PLANS
 JUNE 20, 2017

STRONG STRUCTURAL ENGINEER

<p>ESCA 1738 04</p>	<p>Office of Chief Engineer Bureau de l'ingénieur en chef</p>
<p>BRIDGE 891.50 NEAR LAUFACE, LA BRIDGE REPLACEMENT GENERAL LAYOUT (33 OF 78)</p>	<p>891.50 MACCOMB 091.50</p>
<p>DATE: 06/20/17 DRAWN BY: [Name] CHECKED BY: [Name] DESIGNED BY: [Name]</p>	<p>APR18/891.50.334</p>



60% PLANS
 JUNE 20, 2017
 SENIOR STRUCTURAL ENGINEER

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BRIDGE 891.50
 NEAR WAPLACE LA
BRIDGE REPLACEMENT
 GENERAL LAYOUT (77 OF 78)

Office of Chief Engineer
 Bureau de l'ingénieur en chef

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